

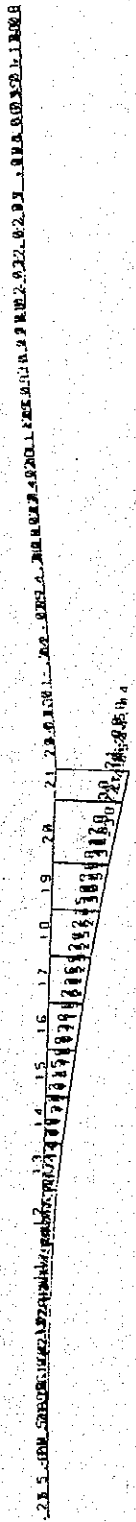
LONG'L WIND LOAD
OF FRAME 1 – CANTILEVER CONSTRUCTION

THE UNIVERSITY OF CHICAGO
LIBRARY

MN/ELEM
MOMENT MZ LN= 2

STRUCTURE DATA

TYPE = SPACE
 NJ = 133
 NM = 132
 NE = 0
 NS = 0
 NRJ = 88
 NL = 2
 XMAX = 96.0
 YMAX = 121.3
 ZMAX = 0



Maximum = 4500.41
 J=133, M=132

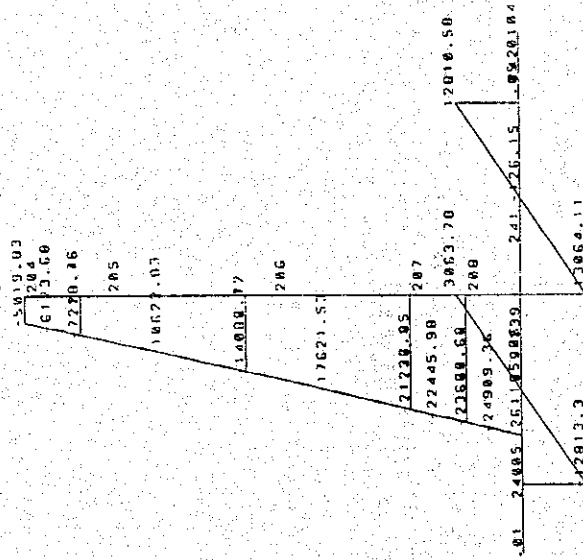
UNIT MET KNS

S T A A O P O S T - P L O T (REV: 27.3)
 DATE: JAN 24, 2000
 TITLE: RUPSA BRIDGE IN KHULNA

MN/ELEM
MOMENT MZ LNE 1

STRUCTURE DATA

TYPE = SPACE
 NJ = 133
 NM = 132
 NE = 0
 NS = 0
 NRJ = 88
 NL = 2
 XMAX = 96.0
 YMAX = 121.3
 ZMAX = .0



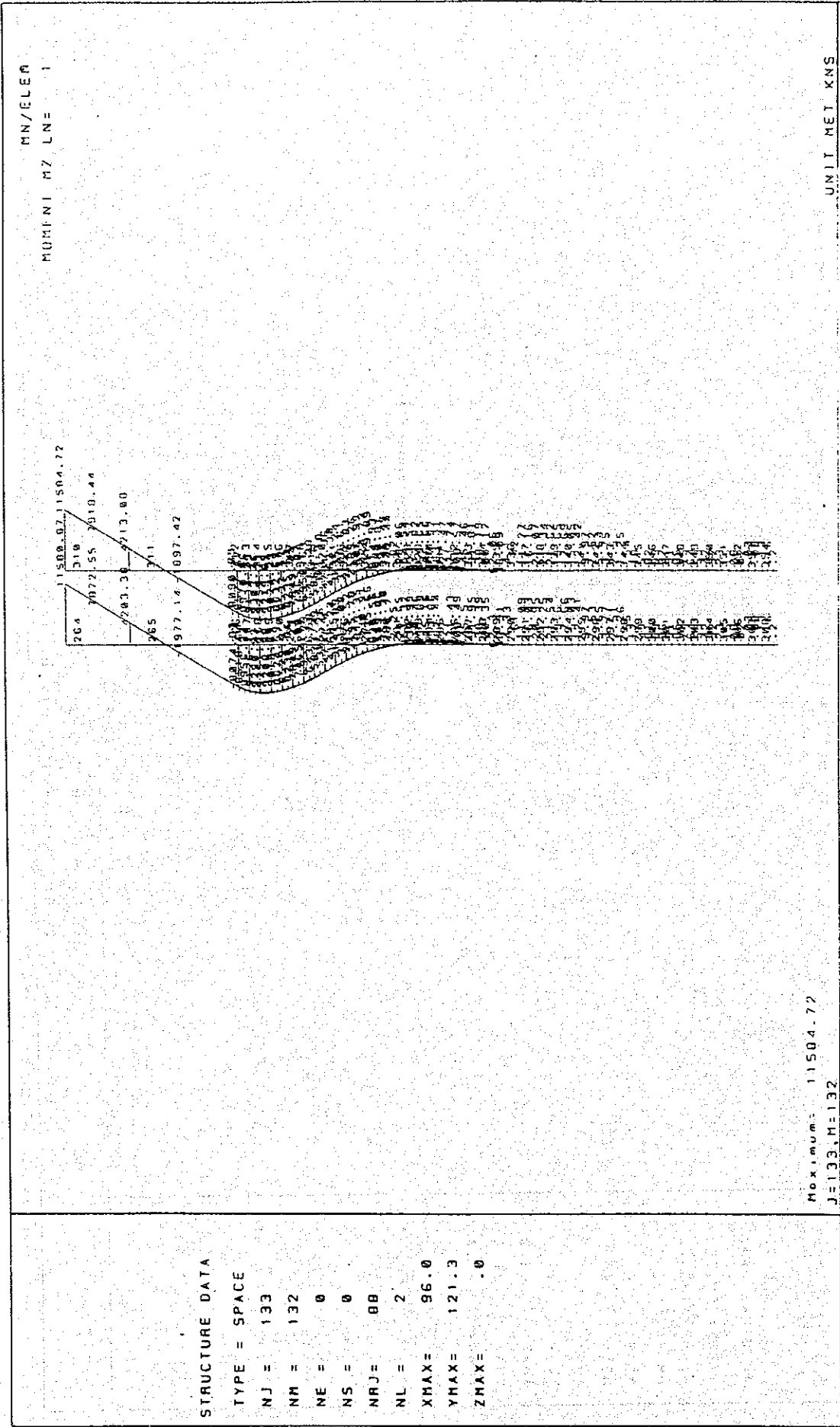
Maximum = 26115.94
 J=133, M=132

UNIT MET KNS

S T A A D P O S T - P L O T (REV: 22.3)

DATE: JAN 24, 2000

TITLE: RUPSA BRIDGE IN KHULNA



MN/ELEM
MOMENT MZ LNE 1

11504.72 11504.72
310
7072.55 7010.44
264
7283.30 7213.80
765
7711
977.14 1097.42

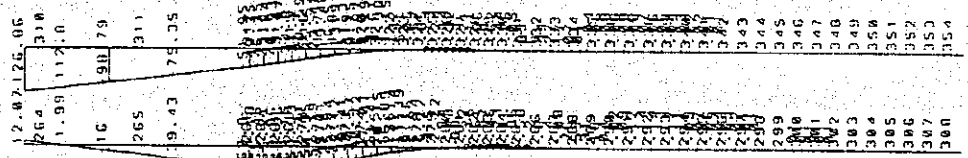
STRUCTURE DATA
TYPE = SPACE
NJ = 133
NM = 132
NE = 0
NS = 0
NRJ = 88
NL = 2
XMAX = 96.0
YMAX = 121.3
ZMAX = .0

Maximum: 11504.72
J=133, M=132

UNIT MET KNS

S T A A D P O S T - P L O T (REV: 22.3)
DATE: JAN 24, 2000
TITLE: RUPSA BRIDGE IN KHULNA

MW/ELEM
MOMENT MZ LNE 2



STRUCTURE DATA
 TYPE = SPACE
 NJ = 133
 NM = 132
 NE = 0
 NS = 0
 NRJ = 88
 NL = 2
 XMAX = 96.0
 YMAX = 121.3
 ZMAX = .0

Maximum = 126.66
 J=133, M=132

UNIT: MET KNS

STAAD.POST - PLOT (RIV: 22-3)
 TITLE: RUPSA BRIDGE IN KHULNA

DATE: JAN 24, 2000

THE STUDY ON THE CONSTRUCTION OF THE BRIDGE OVER THE RIVER RUPSA IN KHULNA, PHASE - 2

Job No. :	Designed by :	Checked by :	Date : January 24, 2000
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LONG'L WIND LOAD

MEMB	LOAD	NODE	AXIAL	SHEAR-Y	SHEAR-Z	TORSION	MOM-Y	MOM-Z
------	------	------	-------	---------	---------	---------	-------	-------

LOADS

LOAD 1 LONG'L WIND LOAD ON SUPERSTRUCTURE

JOINT LOAD
22 F 1472 FZ 983

MEMBER LOAD
204 TO 307 UNI GX 13.9 GY 5.3

LOAD 2 WIND LOAD UPLIFT ON ONE SIDE

MEMBER LOAD
7 TO 20 UNI GY 3.95

THE STUDY ON THE CONSTRUCTION OF THE BRIDGE OVER THE RIVER RUPSA IN KHULNA, PHASE - 2

Job No. : Designed by : Cheked by : Date : January 24, 2000

LONG'L WIND LOAD

MEMB	LOAD	NODE	AXIAL	SHEAR-Y	SHEAR-Z	TORSION	MOM-Y	MOM-Z
7	1	7	-0.35	0.17	-20.4	-0.39	4.61	-0.31
		8	0.35	-0.17	20.4	0.39	-5.47	-0.06
	2	7	-0.01	0.04	0	0	0	0.25
		8	-0.41	-13.87	0	0	0	24.46
8	1	8	-0.21	-0.06	15.42	-0.07	-31.42	0
		9	0.21	0.06	-15.42	0.07	-41.93	-0.25
	2	8	0.42	13.81	0	0	0	-24.33
		9	-0.84	-27.64	0	0	0	96.78
9	1	9	0.49	-0.04	-30.09	0.16	-4.18	0.24
		10	-0.49	0.04	30.09	-0.16	-13.9	0.51
	2	9	0.81	27.66	0	0	0	-96.84
		10	-1.22	-41.49	0	0	0	217.89
10	1	10	-0.19	0.12	5.32	-0.11	-20.98	0.23
		11	0.19	-0.12	-5.32	0.11	-31.33	0.4
	2	10	1.26	41.43	0	0	0	-217.98
		11	-1.67	-55.26	0	0	0	387.12
11	1	11	0.01	-0.11	-3.9	0.25	-15.68	-0.16
		12	-0.01	0.11	3.9	-0.25	-25.25	-0.09
	2	11	1.65	55.26	0	0	0	-387.24
		12	-2.06	-69.09	0	0	0	605.14
12	1	12	0.39	0	9.71	0.32	-27.75	-0.08
		13	-0.39	0	-9.71	-0.32	-39.23	-0.16
	2	12	2.08	69.18	0	0	0	-605.04
		13	-2.49	-83.01	0	0	0	871.4
13	1	13	0.14	0.37	-33.45	0.68	8.26	0.49
		14	-0.14	-0.37	33.45	-0.68	18.62	0.44
	2	13	-2.11	82.97	0	0	0	-871.39
		14	2.41	-94.82	0	0	0	1138.1
14	1	14	0.18	0.22	5.4	0.14	-73.77	0.12
		15	-0.18	-0.22	-5.4	-0.14	-59.47	0.39
	2	14	-2.43	94.83	0	0	0	-1138.15
		15	2.74	-106.68	0	0	0	1440.32
15	1	15	-0.52	-0.25	-15.31	0.05	-17.13	-0.25
		16	0.52	0.25	15.31	-0.05	18.96	0.06
	2	15	-2.8	106.71	0	0	0	-1440.36
		16	3.11	-118.56	0	0	0	1778.26
16	1	16	0.36	0.63	23.07	-0.59	-59.95	0.77
		17	-0.36	-0.63	-23.07	0.59	4.15	0.85
	2	16	-3.16	118.54	0	0	0	-1778.2
		17	3.48	-130.39	0	0	0	2151.7
17	1	17	-0.2	-1.07	30.36	0.5	-42.98	-1.17
		18	0.2	1.07	-30.36	-0.5	-31.21	-0.94
	2	17	-3.48	130.14	0	0	0	-2152.02
		18	3.8	-141.99	0	0	0	2560.45
18	1	18	0.67	0.12	21.7	-0.32	0.4	0.09
		19	-0.67	-0.12	-21.7	0.32	-7.12	-0.05
	2	18	-3.79	142.28	0	0	0	-2560.63
		19	4.1	-154.13	0	0	0	3005.22
19	1	19	0	-0.74	-51.99	0.02	120.24	-1.35
		20	0	0.74	51.99	-0.02	39.09	-1.56
	2	19	-4.02	154.07	0	0	0	-3005.17
		20	4.33	-165.92	0	0	0	3485.38
20	1	20	-0.06	0.11	5.87	0.59	-1.82	0.17
		21	0.06	-0.11	-5.87	-0.59	10.04	0.11
	2	20	-4.19	165.91	0	0	0	-3485.38
		21	4.59	-181.71	0	0	0	4180.83

THE STUDY ON THE CONSTRUCTION OF THE BRIDGE OVER THE RIVER RUPSA IN KHULNA, PHASE - 2

Job No. : Designed by : Cheked by : Date : January 24, 2000

LONG'L WIND LOAD

MEMB	LOAD	NODE	AXIAL	SHEAR-Y	SHEAR-Z	TORSION	MOM-Y	MOM-Z
21	1	21	1.13	-0.07	-8.26	0	75.61	-0.01
		22	-1.13	0.07	8.26	0	68.48	0.2
	2	21	-2.31	181.79	0	0	0	-1180.78
		22	2.31	-181.79	0	0	0	4544.44
22	1	22	-0.14	1.26	-65.41	1.6	-130.33	0.22
		23	0.14	-1.26	65.41	-1.6	31.75	0.21
	2	22	-0.06	-0.04	0	0	0	0
		23	0.06	0.04	0	0	0	-0.03
23	1	23	0.12	0.24	1.34	0.05	46.91	0.4
		24	-0.12	-0.24	-1.34	-0.05	32.24	0.45
	2	23	0	0	0	0	0	0.01
		24	0	0	0	0	0	0
24	1	24	0.33	-0.03	33.2	0.43	24.76	-0.31
		25	-0.33	0.03	-33.2	-0.43	11.79	-0.05
	2	24	-0.01	-0.01	0	0	0	-0.02
		25	0.01	0.01	0	0	0	-0.06
25	1	25	0.09	-0.04	-7.81	0	7.77	-0.11
		26	-0.09	0.04	7.81	0	-26.94	-0.33
	2	25	0.02	-0.03	0	0	0	0.09
		26	-0.02	0.03	0	0	0	0.08
26	1	26	-0.8	0.25	-8.21	-0.2	31.27	0.88
		27	0.8	-0.25	8.21	0.2	24.12	0.69
	2	26	0.01	-0.07	0	0	0	-0.02
		27	-0.01	0.07	0	0	0	0.07
27	1	27	-0.28	1.11	-30.77	-0.29	48.03	1.19
		28	0.28	-1.11	30.77	0.29	92.45	0.68
	2	27	0.01	-0.01	0	0	0	0.02
		28	-0.01	0.01	0	0	0	-0.04
28	1	28	0.14	0.02	12.67	-0.35	-11.34	0.06
		29	-0.14	-0.02	-12.67	0.35	-6.87	-0.05
	2	28	0	-0.02	0	0	0	-0.17
		29	0	0.02	0	0	0	-0.11
29	1	29	0.3	-0.31	11.24	0.26	16.25	-0.34
		30	-0.3	0.31	-11.24	-0.26	-28.73	-0.2
	2	29	0.02	0.02	0	0	0	-0.06
		30	-0.02	-0.02	0	0	0	0
30	1	30	-0.11	-0.81	-22.75	-0.38	23.57	-0.87
		31	0.11	0.81	22.75	0.38	73.36	-0.95
	2	30	0.03	0.01	0	0	0	0
		31	-0.03	-0.01	0	0	0	-0.04
31	1	31	0.14	0.12	29.72	-0.39	-24.53	0.17
		32	-0.14	-0.12	-29.72	0.39	-35.27	0.32
	2	31	0	-0.03	0	0	0	-0.03
		32	0	0.03	0	0	0	-0.02
32	1	32	-0.46	-0.28	-0.68	-0.43	-12.15	-0.68
		33	0.46	0.28	0.68	0.43	14.12	-0.52
	2	32	0	0.03	0	0	0	0.03
		33	0	-0.03	0	0	0	0.02
33	1	33	-0.05	0.05	-2.47	-0.36	1.05	0.4
		34	0.05	-0.05	2.47	0.36	-9.89	-0.08
	2	33	0.01	-0.03	0	0	0	-0.05
		34	-0.01	0.03	0	0	0	-0.05
34	1	34	-0.14	-0.16	-4.19	-0.45	-20.33	-0.01
		35	0.14	0.16	4.19	0.45	-8.8	-0.28
	2	34	-0.02	-0.02	0	0	0	-0.09
		35	0.02	0.02	0	0	0	-0.09

THE STUDY ON THE CONSTRUCTION OF THE BRIDGE OVER THE RIVER RUPSA IN KHULNA, PHASE - 2

Job No. : Designed by : Cheked by : Date : January 24, 2000

LONG'L WIND LOAD

MEMB	LOAD	NODE	AXIAL	SHEAR-Y	SHEAR-Z	TORSION	MOM-Y	MOM-Z
35	1	35	-0.23	-0.12	-17.12	-0.33	31.49	-0.27
		36	0.23	0.12	17.12	0.33	20.61	-0.54
	2	35	0	0	0	0	0	0.01
		36	0	0	0	0	0	0.01
36	1	36	0.05	-0.21	-12.13	-0.12	-8.14	-0.25
		37	-0.05	0.21	12.13	0.12	16.38	-0.53
	2	36	0	0.01	0	0	0	0.1
		37	0	-0.01	0	0	0	0.05
204	1	204	0	1466.59	1022.41	-0.01	3317.68	-5019.83
		205	0	-1487.44	-1030.36	0.01	-4849.62	7216.46
	2	204	-181.77	-0.28	0	0	0	-4544.95
		205	181.77	0.28	0	0	0	4543.73
205	1	205	0.01	1492.84	991.09	-0.01	4824.58	-7228.75
		206	-0.01	-1535.39	-1014.94	0.01	-9332.72	14086.12
	2	205	-181.77	0	0	0	0	-4544.3
		206	181.77	0	0	0	0	4544.23
206	1	206	0.02	1354.95	1013.4	0.01	9336.79	-14087.72
		207	-0.02	-1617.5	-1037.23	-0.01	-13953.72	21226.85
	2	206	-181.77	0.01	0	0	0	-4544.22
		207	181.77	-0.01	0	0	0	4544.16
207	1	207	0.02	1606.43	1053.14	0.09	13936.98	-21237.25
		208	-0.02	-1627.28	-1061.08	-0.09	-15536.06	23660.62
	2	207	-181.77	0.13	0	0	0	-4544
		208	181.77	-0.13	0	0	0	4544.23
239	1	209	-824.24	5447.54	-533.03	-8659.1	2437.92	13063.78
		240	824.24	-5447.54	533.03	8659.1	93.78	12813.3
	2	209	-3.13	557.13	0	0	0	2629.63
		240	3.13	-557.13	0	0	0	16.75
240	1	240	-0.27	-0.05	1.11	0.04	-1.08	-0.05
		241	0.27	0.05	-1.11	-0.04	-1.96	-0.01
	2	240	0	0	0	0	0	0
		241	0	0	0	0	0	0
241	1	209	815.08	-5447.56	513.75	8430.54	-2438.04	-13064.11
		242	-815.08	5447.56	-513.75	-8430.54	-1.83	-12810.58
	2	209	-3.13	-375.36	0	0	0	-1914.5
		242	3.13	375.36	0	0	0	131.54
242	1	242	-0.11	-0.05	-0.72	0.1	0.48	-0.05
		243	0.11	0.05	0.72	-0.1	-0.08	-0.04
	2	242	0	0	0	0	0	0
		243	0	0	0	0	0	0
263	1	240	-5447.52	825.44	531.95	93.67	8659.73	12814.63
		264	5447.52	-825.44	-531.95	-93.67	-9425.78	-11580.14
	2	240	-557.13	3.13	0	0	0	16.75
		264	557.13	-3.13	0	0	0	-12.07
264	1	264	-5447.54	824.07	532.96	93.67	9423.77	11580.87
		265	5447.54	-824.07	-532.96	-93.67	-14030.97	-4203.39
	2	264	-557.13	3.12	0	0	0	12.07
		265	557.13	-3.12	0	0	0	16
265	1	265	-5447.54	824.07	532.98	93.67	14030.96	4203.38
		266	5447.54	-824.07	-532.98	-93.67	-21812.47	8074.92
	2	265	-557.13	3.12	0	0	0	-16
		266	557.13	-3.12	0	0	0	62.8
266	1	266	-5447.54	591.63	137.08	93.67	21812.66	-8074.91
		267	5447.54	-591.63	-137.08	-93.67	-22005.92	8956.33
	2	266	-557.13	1.77	0	0	0	-62.8
		267	557.13	-1.77	0	0	0	65.45

THE STUDY ON THE CONSTRUCTION OF THE BRIDGE OVER THE RIVER RUPSA IN KHULNA, PHASE - 2

Job No. : Designed by : Cheked by : Date : January 24, 2000

LONG'L WIND LOAD

MEMB	LOAD	NODE	AXIAL	SHEAR-Y	SHEAR-Z	TORSION	MOM-Y	MOM-Z
267	1	267	-5447.54	226.46	-460.45	93.67	22006.14	-8956.27
		268	5447.54	-226.46	460.45	-93.67	-21305.27	9290.53
	2	267	-557.13	-0.32	0	0	0	-65.45
		268	557.13	0.32	0	0	0	64.96
268	1	268	-5447.55	-51.2	-892.88	93.67	21305.27	-9290.37
		269	5447.55	51.2	892.88	-93.67	-19957.55	9208.98
	2	268	-557.13	-1.87	0	0	0	-64.96
		269	557.13	1.87	0	0	0	62.15
269	1	269	-5447.56	-253.72	-1189.19	93.67	19957.52	-9208.96
		270	5447.56	253.72	1189.19	-93.67	-18166.91	8824.53
	2	269	-557.13	-2.96	0	0	0	-62.15
		270	557.13	2.96	0	0	0	57.71
270	1	270	-5447.56	-393.66	-1376.86	93.67	18166.95	-8824.53
		271	5447.56	393.66	1376.86	-93.67	-16096.51	8230.86
	2	270	-557.13	-3.68	0	0	0	-57.71
		271	557.13	3.68	0	0	0	52.19
271	1	271	-5447.55	-508.91	-1510.33	93.67	16096.57	-8230.91
		272	5447.55	508.91	1510.33	-93.67	-13827.23	7465.07
	2	271	-557.13	-4.23	0	0	0	-52.19
		272	557.13	4.23	0	0	0	45.84
272	1	272	-5447.54	-682.44	-1654.06	93.67	13827.23	-7465.06
		273	5447.54	682.44	1654.06	-93.67	-11343.46	6439.59
	2	272	-557.13	-4.95	0	0	0	-45.84
		273	557.13	4.95	0	0	0	38.41
273	1	273	-5447.54	-755.04	-1648.41	93.67	11343.47	-6439.58
		274	5447.54	755.04	1648.41	-93.67	-8869.15	5305.75
	2	273	-557.13	-5.12	0	0	0	-38.41
		274	557.13	5.12	0	0	0	30.72
274	1	274	-5447.54	-757.8	-1517.04	93.67	8869.18	-5305.76
		275	5447.54	757.8	1517.04	-93.67	-6592.65	4168.23
	2	274	-557.13	-4.88	0	0	0	-30.72
		275	557.13	4.88	0	0	0	23.41
275	1	275	-5447.54	-701.8	-1316.32	93.67	6592.68	-4168.24
		276	5447.54	701.8	1316.32	-93.67	-4617.79	3115.08
	2	275	-557.13	-4.34	0	0	0	-23.41
		276	557.13	4.34	0	0	0	16.89
276	1	276	-5447.56	-612.73	-1087.22	93.67	4617.8	-3115.07
		277	5447.56	612.73	1087.22	-93.67	-2986.97	2195.79
	2	276	-557.13	-3.67	0	0	0	-16.89
		277	557.13	3.67	0	0	0	11.39
277	1	277	-5447.53	-509.9	-858.39	93.67	2986.97	-2195.78
		278	5447.53	509.9	858.39	-93.67	-1699.65	1430.91
	2	277	-557.13	-2.96	0	0	0	-11.39
		278	557.13	2.96	0	0	0	6.94
278	1	278	-5447.54	-406.98	-648.61	93.67	1699.68	-1430.9
		279	5447.54	406.98	648.61	-93.67	-727.16	820.56
	2	278	-557.13	-2.29	0	0	0	-6.94
		279	557.13	2.29	0	0	0	3.5
279	1	279	-5447.55	-312.92	-468.59	93.67	727.2	-820.55
		280	5447.55	312.92	468.59	-93.67	-24.78	351.38
	2	279	-557.13	-1.71	0	0	0	-3.5
		280	557.13	1.71	0	0	0	0.94
280	1	280	-5447.55	-232.9	-323.13	93.67	24.79	-351.37
		281	5447.55	232.9	323.13	-93.67	459.4	2.25
	2	280	-557.13	-1.22	0	0	0	-0.94
		281	557.13	1.22	0	0	0	-0.89

THE STUDY ON THE CONSTRUCTION OF THE BRIDGE OVER THE RIVER RUPSA IN KHULNA, PHASE - 2

Job No. : Designed by : Cheked by : Date : January 24, 2000

LONG'L WIND LOAD

MEMB	LOAD	NODE	AXIAL	SHEAR-Y	SHEAR-Z	TORSION	MOM-Y	MOM-Z
281	1	281	-5447.55	-169.2	-212.57	93.67	-459.42	-2.25
		282	5447.55	169.2	212.57	-93.67	777.82	-251.32
	2	281	-557.13	-0.84	0	0	0	0.89
		282	557.13	0.84	0	0	0	-2.15
282	1	282	-5447.54	-106.23	-108.27	93.67	-777.82	251.32
		283	5447.54	106.23	108.27	-93.67	939.84	-410.46
	2	282	-557.13	-0.48	0	0	0	2.15
		283	557.13	0.48	0	0	0	-2.87
283	1	283	-5447.54	-63.24	-40.79	93.67	-939.84	410.46
		284	5447.54	63.24	40.79	-93.67	1000.71	-505.16
	2	283	-557.13	-0.24	0	0	0	2.87
		284	557.13	0.24	0	0	0	-3.23
284	1	284	-5447.54	15.06	73.03	93.67	-1000.71	505.16
		285	5447.54	-15.06	-73.03	-93.67	890.95	-482.44
	2	284	-557.13	0.18	0	0	0	3.23
		285	557.13	-0.18	0	0	0	-2.96
285	1	285	-5447.53	55.45	123.38	93.67	-890.95	482.44
		286	5447.53	-55.45	-123.38	-93.67	644.02	-371.45
	2	285	-557.13	0.38	0	0	0	2.96
		286	557.13	-0.38	0	0	0	-2.2
286	1	286	-5447.55	62.94	121.75	93.67	-644.02	371.45
		287	5447.55	-62.94	-121.75	-93.67	400.42	-245.5
	2	286	-557.13	0.4	0	0	0	2.2
		287	557.13	-0.4	0	0	0	-1.4
287	1	287	-5447.53	53.54	96.12	93.67	-400.42	245.5
		288	5447.53	-53.54	-96.12	-93.67	208.15	-138.39
	2	287	-557.13	0.32	0	0	0	1.4
		288	557.13	-0.32	0	0	0	-0.76
288	1	288	-5447.53	38.04	64.24	93.67	-208.15	138.39
		289	5447.53	-38.04	-64.24	-93.67	79.69	-62.3
	2	288	-557.13	0.22	0	0	0	0.76
		289	557.13	-0.22	0	0	0	-0.31
289	1	289	-5447.54	23.99	38.05	93.67	-79.69	62.3
		290	5447.54	-23.99	-38.05	-93.67	3.63	-14.33
	2	289	-557.13	0.13	0	0	0	0.31
		290	557.13	-0.13	0	0	0	-0.04
290	1	290	-5447.55	12.72	18.32	93.67	-3.63	14.33
		291	5447.55	-12.72	-18.32	-93.67	-32.98	11.09
	2	290	-557.13	0.07	0	0	0	0.04
		291	557.13	-0.07	0	0	0	0.09
291	1	291	-5447.54	4.93	5.46	93.67	32.98	-11.09
		292	5447.54	-4.93	-5.46	-93.67	-43.88	20.95
	2	291	-557.13	0.02	0	0	0	-0.09
		292	557.13	-0.02	0	0	0	0.14
292	1	292	-5447.54	0.3	-1.67	93.67	43.88	-20.95
		293	5447.54	-0.3	1.67	-93.67	-40.54	21.54
	2	292	-557.13	0	0	0	0	-0.14
		293	557.13	0	0	0	0	0.13
293	1	293	-5447.54	-1.94	-4.71	93.67	40.54	-21.54
		294	5447.54	1.94	4.71	-93.67	-31.11	17.66
	2	293	-557.13	-0.01	0	0	0	-0.13
		294	557.13	0.01	0	0	0	0.11
294	1	294	-5447.55	-2.62	-5.23	93.67	31.11	-17.66
		295	5447.55	2.62	5.23	-93.67	-20.64	12.41
	2	294	-557.13	-0.02	0	0	0	-0.11
		295	557.13	0.02	0	0	0	0.07

THE STUDY ON THE CONSTRUCTION OF THE BRIDGE OVER THE RIVER RUPSA IN KHULNA, PHASE - 2

Job No. : Designed by : Cheked by : Date : January 24, 2000

LONG'L WIND LOAD

MEMB	LOAD	NODE	AXIAL	SHEAR-Y	SHEAR-Z	TORSION	MOM-Y	MOM-Z
295	1	295	-5447.54	-2.44	-4.47	93.67	20.64	-12.41
		296	5447.54	2.44	4.47	-93.67	-11.71	7.53
	2	295	-557.13	-0.01	0	0	0	-0.07
		296	557.13	0.01	0	0	0	0.04
296	1	296	-5447.54	-1.88	-3.24	93.67	11.71	-7.53
		297	5447.54	1.88	3.24	-93.67	-5.22	3.77
	2	296	-557.13	-0.01	0	0	0	-0.04
		297	557.13	0.01	0	0	0	0.02
297	1	297	-5447.54	-1.25	-2.04	93.67	5.22	-3.77
		298	5447.54	1.25	2.04	-93.67	-1.15	1.26
	2	297	-557.13	-0.01	0	0	0	-0.02
		298	557.13	0.01	0	0	0	0.01
298	1	298	-5447.54	-0.71	-1.07	93.67	1.15	-1.26
		299	5447.54	0.71	1.07	-93.67	1	-0.17
	2	298	-557.13	0	0	0	0	-0.01
		299	557.13	0	0	0	0	0
299	1	299	-5447.54	-0.32	-0.41	93.67	-1	0.17
		300	5447.54	0.32	0.41	-93.67	1.82	-0.81
	2	299	-557.13	0	0	0	0	0
		300	557.13	0	0	0	0	-0.01
300	1	300	-5447.54	-0.08	-0.02	93.67	-1.82	0.81
		301	5447.54	0.08	0.02	-93.67	1.87	-0.96
	2	300	-557.13	0	0	0	0	0.01
		301	557.13	0	0	0	0	-0.01
301	1	301	-5447.54	0.06	0.17	93.67	-1.87	0.96
		302	5447.54	-0.06	-0.17	-93.67	1.53	-0.85
	2	301	-557.13	0	0	0	0	0.01
		302	557.13	0	0	0	0	-0.01
302	1	302	-5447.54	0.11	0.22	93.67	-1.53	0.85
		303	5447.54	-0.11	-0.22	-93.67	1.08	-0.64
	2	302	-557.13	0	0	0	0	0.01
		303	557.13	0	0	0	0	0
303	1	303	-5447.54	0.11	0.21	93.67	-1.08	0.64
		304	5447.54	-0.11	-0.21	-93.67	0.66	-0.42
	2	303	-557.13	0	0	0	0	0
		304	557.13	0	0	0	0	0
304	1	304	-5447.54	0.09	0.17	93.67	-0.66	0.42
		305	5447.54	-0.09	-0.17	-93.67	0.33	-0.23
	2	304	-557.13	0	0	0	0	0
		305	557.13	0	0	0	0	0
305	1	305	-5447.54	0.07	0.12	93.67	-0.33	0.23
		306	5447.54	-0.07	-0.12	-93.67	0.09	-0.08
	2	305	-557.13	0	0	0	0	0
		306	557.13	0	0	0	0	0
306	1	306	-5447.54	0.06	0.09	93.67	-0.09	0.08
		307	5447.54	-0.06	-0.09	-93.67	-0.09	0.03
	2	306	-557.13	0	0	0	0	0
		307	557.13	0	0	0	0	0
307	1	307	-5447.54	0.05	0.07	93.67	0.09	-0.03
		308	5447.54	-0.05	-0.07	-93.67	-0.24	0.13
	2	307	-557.13	0	0	0	0	0
		308	557.13	0	0	0	0	0
308	1	308	-5447.54	0.05	0.07	93.67	0.24	-0.13
		309	5447.54	-0.05	-0.07	-93.67	-0.35	0.2
	2	308	-557.13	0	0	0	0	0
		309	557.13	0	0	0	0	0

THE STUDY ON THE CONSTRUCTION OF THE BRIDGE OVER THE RIVER RUPSA IN KHULNA, PHASE - 2

Job No. : Designed by : Cheked by : Date : January 24, 2000

LONG'L WIND LOAD

MEMB	LOAD	NODE	AXIAL	SHEAR-Y	SHEAR-Z	TORSION	MOM-Y	MOM-Z
309	1	242	5447.53	815.47	512.94	-2.12	8430.14	12810.69
		310	-5447.53	-815.47	-512.94	2.12	-9233.95	-11584.58
	2	242	375.36	-3.12	0	0	0	-131.53
		310	-375.36	3.12	0	0	0	126.87
310	1	310	5447.54	814.73	513.64	-1.95	9253.73	11584.72
		311	-5447.54	-814.73	-513.64	1.95	-14048.11	-4213.09
	2	310	375.36	-3.12	0	0	0	-126.86
		311	-375.36	3.12	0	0	0	98.79
311	1	311	5447.54	814.73	513.62	-1.94	14048.02	4213.06
		312	-5447.54	-814.73	-513.62	1.94	-21965.52	8090.63
	2	311	375.36	-3.12	0	0	0	-98.79
		312	-375.36	3.12	0	0	0	51.95
312	1	312	5447.55	582.57	116.99	-1.94	21965.46	-8090.75
		314	-5447.55	-582.57	-116.99	1.94	-22153.17	8970.86
	2	312	375.36	-3.47	0	0	0	-51.95
		314	-375.36	3.47	0	0	0	46.75
313	1	314	5447.55	217.72	-481.6	-1.94	22153.16	-8971.16
		315	-5447.55	-217.72	481.6	1.94	-21440.88	9303.31
	2	314	375.36	-3.88	0	0	0	-46.75
		315	-375.36	3.88	0	0	0	40.93
314	1	315	5447.52	-58.78	-913.95	-1.94	21440.97	-9303.46
		316	-5447.52	58.78	913.95	1.94	-20078.38	9219.89
	2	315	375.36	-4.09	0	0	0	-40.93
		316	-375.36	4.09	0	0	0	34.8
315	1	316	5447.52	-260.88	-1210.32	-1.94	20078.33	-9219.95
		317	-5447.52	260.88	1210.32	1.94	-18269.62	8832.55
	2	316	375.36	-4.14	0	0	0	-34.8
		317	-375.36	4.14	0	0	0	28.59
316	1	317	5447.51	-400.14	-1397.3	-1.94	18269.6	-8832.67
		318	-5447.51	400.14	1397.3	1.94	-16178.85	8235.53
	2	317	375.36	-4.1	0	0	0	-28.59
		318	-375.36	4.1	0	0	0	22.44
317	1	318	5447.51	-514.76	-1529.8	-1.94	16178.88	-8235.51
		319	-5447.51	514.76	1529.8	1.94	-13887.99	7465.8
	2	318	375.36	-3.95	0	0	0	-22.44
		319	-375.36	3.95	0	0	0	16.52
318	1	319	5447.56	-686.86	-1670.6	-1.94	13888	-7465.77
		320	-5447.56	686.86	1670.6	1.94	-11384.76	6437.29
	2	319	375.36	-3.45	0	0	0	-16.52
		320	-375.36	3.45	0	0	0	11.34
319	1	320	5447.52	-758.25	-1662	-1.94	11384.75	-6437.29
		321	-5447.52	758.25	1662	1.94	-8893.46	5301.19
	2	320	375.36	-2.91	0	0	0	-11.34
		321	-375.36	2.91	0	0	0	6.97
320	1	321	5447.51	-759.77	-1527.1	-1.94	8893.44	-5301.18
		322	-5447.51	759.77	1527.1	1.94	-6603.74	4162.34
	2	321	375.36	-2.24	0	0	0	-6.97
		322	-375.36	2.24	0	0	0	3.61
321	1	322	5447.57	-702.8	-1323.35	-1.94	6603.73	-4162.35
		323	-5447.57	702.8	1323.35	1.94	-4619.08	3108.61
	2	322	375.36	-1.63	0	0	0	-3.61
		323	-375.36	1.63	0	0	0	1.15
322	1	323	5447.56	-613.03	-1091.7	-1.94	4619.08	-3108.61
		324	-5447.56	613.03	1091.7	1.94	-2981.54	2189.26
	2	323	375.36	-1.12	0	0	0	-1.15
		324	-375.36	1.12	0	0	0	-0.52

THE STUDY ON THE CONSTRUCTION OF THE BRIDGE OVER THE RIVER RUPSA IN KHULNA, PHASE - 2

Job No. : Designed by : Cheked by : Date : January 24, 2000

LONG'L WIND LOAD

MEMB	LOAD	NODE	AXIAL	SHEAR-Y	SHEAR-Z	TORSION	MOM-Y	MOM-Z
323	1	324	5447.55	-509.7	-860.78	-1.94	2981.51	-2189.26
		325	-5447.55	509.7	860.78	1.94	-1690.06	1424.72
	2	324	375.36	-0.7	0	0	0	0.52
		325	-375.36	0.7	0	0	0	-1.57
324	1	325	5447.55	-406.45	-649.51	-1.94	1690.05	-1424.73
		326	-5447.55	406.45	649.51	1.94	-715.36	814.93
	2	325	375.36	-0.38	0	0	0	1.57
		326	-375.36	0.38	0	0	0	-2.13
325	1	326	5447.52	-312.17	-468.41	-1.94	715.41	-814.93
		327	-5447.52	312.17	468.41	1.94	-12.31	346.49
	2	326	375.36	-0.14	0	0	0	2.13
		327	-375.36	0.14	0	0	0	-2.34
326	1	327	5447.51	-232.04	-322.18	-1.94	12.32	-346.47
		328	-5447.51	232.04	322.18	1.94	471.44	-1.8
	2	327	375.36	0.02	0	0	0	2.34
		328	-375.36	-0.02	0	0	0	-2.3
327	1	328	5447.55	-168.31	-211.21	-1.94	-471.43	1.82
		329	-5447.55	168.31	211.21	1.94	788.71	-254.54
	2	328	375.36	0.13	0	0	0	2.3
		329	-375.36	-0.13	0	0	0	-2.11
328	1	329	5447.53	-105.38	-106.59	-1.94	-788.7	254.54
		330	-5447.53	105.38	106.59	1.94	948.98	-412.82
	2	329	375.36	0.21	0	0	0	2.11
		330	-375.36	-0.21	0	0	0	-1.8
329	1	330	5447.55	-62.44	-39.03	-1.94	-948.98	412.82
		331	-5447.55	62.44	39.03	1.94	1007.83	-506.66
	2	330	375.36	0.24	0	0	0	1.8
		331	-375.36	-0.24	0	0	0	-1.44
330	1	331	5447.55	15.65	74.68	-1.94	-1007.83	506.66
		313	-5447.55	-15.65	-74.68	1.94	896.04	-483.31
	2	331	375.36	0.26	0	0	0	1.44
		313	-375.36	-0.26	0	0	0	-1.05
331	1	313	5447.53	55.85	124.7	-1.94	-896.04	483.31
		333	-5447.53	-55.85	-124.7	1.94	646.82	-371.74
	2	313	375.36	0.23	0	0	0	1.05
		333	-375.36	-0.23	0	0	0	-0.6
332	1	333	5447.55	63.17	122.66	-1.94	-646.82	371.74
		334	-5447.55	-63.17	-122.66	1.94	401.57	-245.46
	2	333	375.36	0.17	0	0	0	0.6
		334	-375.36	-0.17	0	0	0	-0.27
333	1	334	5447.54	53.65	96.66	-1.94	-401.57	245.46
		335	-5447.54	-53.65	-96.66	1.94	208.28	-138.19
	2	334	375.36	0.1	0	0	0	0.27
		335	-375.36	-0.1	0	0	0	-0.07
334	1	335	5447.54	38.06	64.48	-1.94	-208.28	138.19
		336	-5447.54	-38.06	-64.48	1.94	79.32	-62.06
	2	335	375.36	0.05	0	0	0	0.07
		336	-375.36	-0.05	0	0	0	0.04
335	1	336	5447.54	23.97	38.11	-1.94	-79.32	62.06
		337	-5447.54	-23.97	-38.11	1.94	3.07	-14.1
	2	336	375.36	0.02	0	0	0	-0.04
		337	-375.36	-0.02	0	0	0	0.08
336	1	337	5447.55	12.68	18.28	-1.94	-3.07	14.1
		338	-5447.55	-12.68	-18.28	1.94	-33.52	11.27
	2	337	375.36	0	0	0	0	-0.08
		338	-375.36	0	0	0	0	0.09

THE STUDY ON THE CONSTRUCTION OF THE BRIDGE OVER THE RIVER RUPSA IN KHULNA, PHASE - 2

Job No. : Designed by : Cheked by : Date : January 24, 2000

LONG'L WIND LOAD

MEMB	LOAD	NODE	AXIAL	SHEAR-Y	SHEAR-Z	TORSION	MOM-Y	MOM-Z
337	1	338	5447.54	4.89	5.38	-1.94	33.52	-11.27
		339	-5447.54	-4.89	-5.38	1.94	-44.31	21.07
	2	338	375.36	-0.01	0	0	0	-0.09
		339	-375.36	0.01	0	0	0	0.07
338	1	339	5447.53	0.27	-1.75	-1.94	44.31	-21.07
		340	-5447.53	-0.27	1.75	1.94	-40.83	21.62
	2	339	375.36	-0.01	0	0	0	-0.07
		340	-375.36	0.01	0	0	0	0.05
339	1	340	5447.54	-1.97	-4.78	-1.94	40.83	-21.62
		341	-5447.54	1.97	4.78	1.94	-31.29	17.69
	2	340	375.36	-0.01	0	0	0	-0.05
		341	-375.36	0.01	0	0	0	0.03
340	1	341	5447.54	-2.64	-5.28	-1.94	31.29	-17.69
		342	-5447.54	2.64	5.28	1.94	-20.72	12.42
	2	341	375.36	-0.01	0	0	0	-0.03
		342	-375.36	0.01	0	0	0	0.02
341	1	342	5447.54	-2.45	-4.5	-1.94	20.72	-12.42
		343	-5447.54	2.45	4.5	1.94	-11.73	7.52
	2	342	375.36	-0.01	0	0	0	-0.02
		343	-375.36	0.01	0	0	0	0.01
342	1	343	5447.54	-1.88	-3.26	-1.94	11.73	-7.52
		344	-5447.54	1.88	3.26	1.94	-5.21	3.75
	2	343	375.36	0	0	0	0	-0.01
		344	-375.36	0	0	0	0	0
343	1	344	5447.54	-1.25	-2.04	-1.94	5.21	-3.75
		345	-5447.54	1.25	2.04	1.94	-1.12	1.25
	2	344	375.36	0	0	0	0	0
		345	-375.36	0	0	0	0	0
344	1	345	5447.54	-0.71	-1.07	-1.94	1.12	-1.25
		346	-5447.54	0.71	1.07	1.94	1.03	-0.18
	2	345	375.36	0	0	0	0	0
		346	-375.36	0	0	0	0	0
345	1	346	5447.54	-0.32	-0.41	-1.94	-1.03	0.18
		347	-5447.54	0.32	0.41	1.94	1.85	-0.82
	2	346	375.36	0	0	0	0	0
		347	-375.36	0	0	0	0	0
346	1	347	5447.54	-0.07	-0.02	-1.94	-1.85	0.82
		348	-5447.54	0.07	0.02	1.94	1.88	-0.97
	2	347	375.36	0	0	0	0	0
		348	-375.36	0	0	0	0	0
347	1	348	5447.54	0.06	0.17	-1.94	-1.88	0.97
		349	-5447.54	-0.06	-0.17	1.94	1.54	-0.85
	2	348	375.36	0	0	0	0	0
		349	-375.36	0	0	0	0	0
348	1	349	5447.54	0.11	0.23	-1.94	-1.54	0.85
		350	-5447.54	-0.11	-0.23	1.94	1.09	-0.64
	2	349	375.36	0	0	0	0	0
		350	-375.36	0	0	0	0	0
349	1	350	5447.54	0.11	0.21	-1.94	-1.09	0.64
		351	-5447.54	-0.11	-0.21	1.94	0.67	-0.42
	2	350	375.36	0	0	0	0	0
		351	-375.36	0	0	0	0	0
350	1	351	5447.54	0.09	0.17	-1.94	-0.67	0.42
		352	-5447.54	-0.09	-0.17	1.94	0.33	-0.23
	2	351	375.36	0	0	0	0	0
		352	-375.36	0	0	0	0	0

THE STUDY ON THE CONSTRUCTION OF THE BRIDGE OVER THE RIVER RUPSA IN KHULNA, PHASE - 2

Job No. : Designed by : Cheked by : Date : January 24, 2000

LONG'L WIND LOAD

MEMB	LOAD	NODE	AXIAL	SHEAR-Y	SHEAR-Z	TORSION	MOM-Y	MOM-Z
351	1	352	5447.54	0.07	0.12	-1.94	-0.33	0.23
		353	-5447.54	-0.07	-0.12	1.94	0.08	-0.08
	2	352	375.36	0	0	0	0	0
		353	-375.36	0	0	0	0	0
352	1	353	5447.54	0.06	0.09	-1.94	-0.08	0.08
		354	-5447.54	-0.06	-0.09	1.94	-0.1	0.03
	2	353	375.36	0	0	0	0	0
		354	-375.36	0	0	0	0	0
353	1	354	5447.54	0.05	0.07	-1.94	0.1	-0.03
		332	-5447.54	-0.05	-0.07	1.94	-0.24	0.13
	2	354	375.36	0	0	0	0	0
		332	-375.36	0	0	0	0	0
354	1	332	5447.54	0.05	0.07	-1.94	0.24	-0.13
		355	-5447.54	-0.05	-0.07	1.94	-0.35	0.2
	2	332	375.36	0	0	0	0	0
		355	-375.36	0	0	0	0	0

FRAME - 2 RESULT

**GEOMETRIC INPUT
OF FRAME 2**

MN/ELEM

1 2 3 4 5 6 7 8 9 10 11 12 13 14 15 16 17 18 19 20 21 22 23 24 25 26 27 28 29 30 31 32 33 34 35 36

STRUCTURE DATA

TYPE = SPACE
 NJ = 139
 NH = 130
 NE = 0
 NS = 0
 NRJ = 89
 NL = 2
 XMAX = 118.0
 YMAX = 121.3
 ZMAX = .0

264	310
265	311
266	312
267	313
268	314
269	315
270	316
271	317
272	318
273	319
274	320
275	321
276	322
277	323
278	324
279	325
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300	346
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302	348
303	349
304	350
305	351
306	352
307	353
308	354

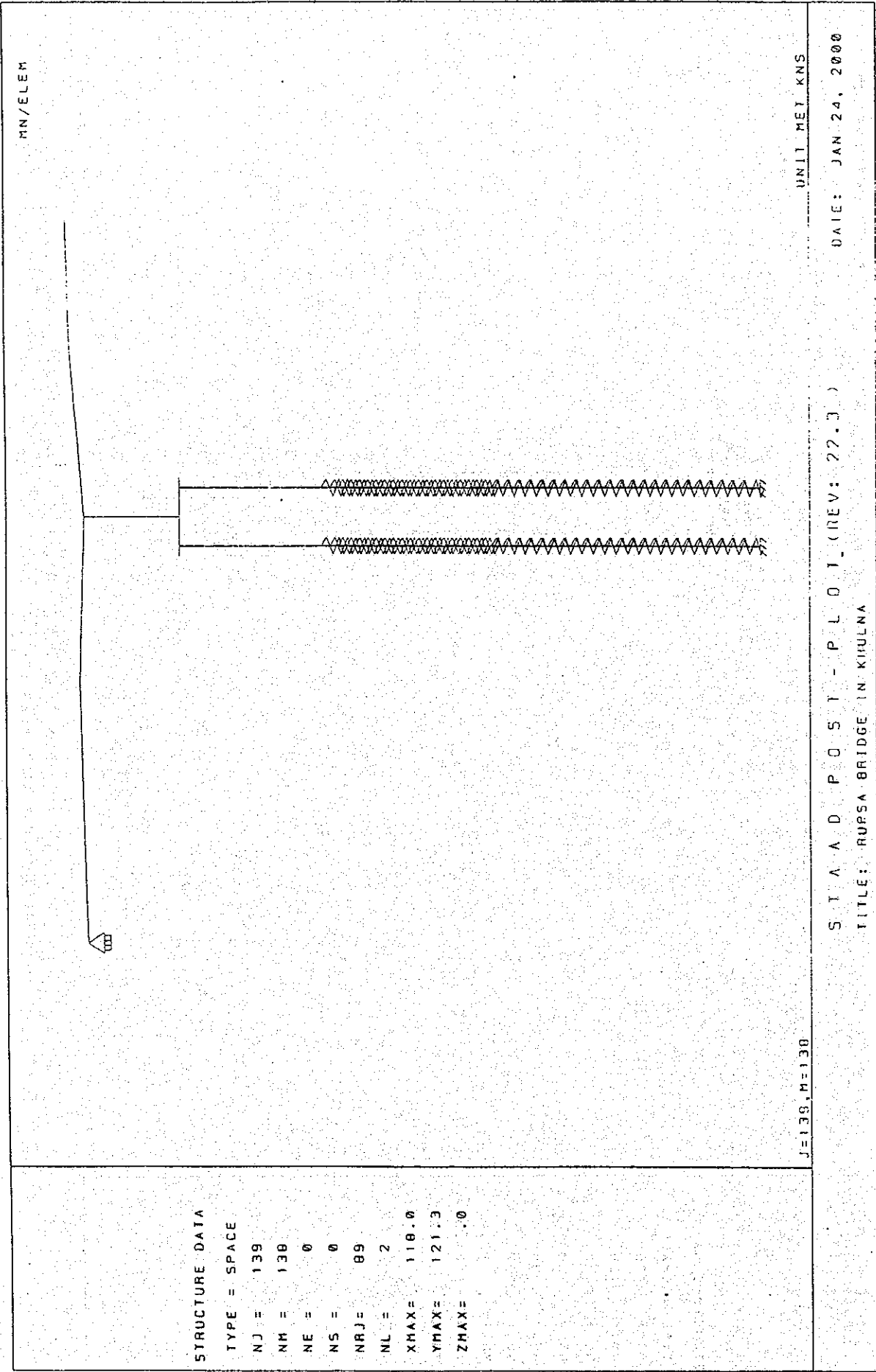
J=139, M=138

UNIT MET KNS

S T A A D P O S T P L O I (REV: 22.3)

DATE: JAN 24, 2000

TITLE: RUPSA BRIDGE IN KHULNA



STRUCTURE DATA
 TYPE = SPACE
 NJ = 139
 NH = 130
 NE = 0
 NS = 0
 NRJ = 89
 NL = 2
 XMAX = 118.0
 YMAX = 121.3
 ZMAX = .0

J=139, M=130

STAAD.POST - PLOT (REV: 22.3)

TITLE: RUPSA BRIDGE IN KIWUNA

DATE: JAN 24, 2000

UNIT MET KNS

THE STUDY ON THE CONSTRUCTION OF THE BRIDGE OVER THE RIVER RUPSA IN KHULNA, PHASE - 2

Job No. :	Designed by :	Checked by :	Date : 1/24/00
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GEOMETRIC INPUT

<p>.....</p> <p>STAAD - III Revision 22.3 Proprietary Program of Research Engineers, Inc. Date= JAN 24, 2000 Time= 14:25:38</p> <p>USER ID: Pacific Consultants Internationa *</p> <p>.....</p> <p>1. STAAD SPACE RUPSA BRIDGE IN KHULNA 2. INPUT WIDTH 72 3. UNIT METER KNS 4. JOINT COORDINATES</p>	<table border="0" style="width: 100%; border-collapse: collapse;"> <tr><td>57.</td><td>269</td><td>65.250</td><td>-29.500</td><td>.000</td></tr> <tr><td>58.</td><td>270</td><td>65.250</td><td>-31.000</td><td>.000</td></tr> <tr><td>59.</td><td>271</td><td>65.250</td><td>-32.500</td><td>.000</td></tr> <tr><td>60.</td><td>272</td><td>65.250</td><td>-34.000</td><td>.000</td></tr> <tr><td>61.</td><td>273</td><td>65.250</td><td>-35.500</td><td>.000</td></tr> <tr><td>62.</td><td>274</td><td>65.250</td><td>-37.000</td><td>.000</td></tr> <tr><td>63.</td><td>275</td><td>65.250</td><td>-38.500</td><td>.000</td></tr> <tr><td>64.</td><td>276</td><td>65.250</td><td>-40.000</td><td>.000</td></tr> <tr><td>65.</td><td>277</td><td>65.250</td><td>-41.500</td><td>.000</td></tr> <tr><td>66.</td><td>278</td><td>65.250</td><td>-43.000</td><td>.000</td></tr> <tr><td>67.</td><td>279</td><td>65.250</td><td>-44.500</td><td>.000</td></tr> <tr><td>68.</td><td>280</td><td>65.250</td><td>-46.000</td><td>.000</td></tr> <tr><td>69.</td><td>281</td><td>65.250</td><td>-47.500</td><td>.000</td></tr> <tr><td>70.</td><td>282</td><td>65.250</td><td>-49.000</td><td>.000</td></tr> <tr><td>71.</td><td>283</td><td>65.250</td><td>-50.500</td><td>.000</td></tr> <tr><td>72.</td><td>284</td><td>65.250</td><td>-52.000</td><td>.000</td></tr> <tr><td>73.</td><td>285</td><td>65.250</td><td>-53.500</td><td>.000</td></tr> <tr><td>74.</td><td>286</td><td>65.250</td><td>-55.000</td><td>.000</td></tr> <tr><td>75.</td><td>287</td><td>65.250</td><td>-57.500</td><td>.000</td></tr> <tr><td>76.</td><td>288</td><td>65.250</td><td>-59.500</td><td>.000</td></tr> <tr><td>77.</td><td>289</td><td>65.250</td><td>-61.500</td><td>.000</td></tr> <tr><td>78.</td><td>290</td><td>65.250</td><td>-63.500</td><td>.000</td></tr> <tr><td>79.</td><td>291</td><td>65.250</td><td>-65.500</td><td>.000</td></tr> <tr><td>80.</td><td>292</td><td>65.250</td><td>-67.500</td><td>.000</td></tr> <tr><td>81.</td><td>293</td><td>65.250</td><td>-69.500</td><td>.000</td></tr> <tr><td>82.</td><td>294</td><td>65.250</td><td>-71.500</td><td>.000</td></tr> <tr><td>83.</td><td>295</td><td>65.250</td><td>-73.500</td><td>.000</td></tr> <tr><td>84.</td><td>296</td><td>65.250</td><td>-75.500</td><td>.000</td></tr> <tr><td>85.</td><td>297</td><td>65.250</td><td>-77.500</td><td>.000</td></tr> <tr><td>86.</td><td>298</td><td>65.250</td><td>-79.500</td><td>.000</td></tr> <tr><td>87.</td><td>299</td><td>65.250</td><td>-81.500</td><td>.000</td></tr> <tr><td>88.</td><td>300</td><td>65.250</td><td>-83.500</td><td>.000</td></tr> <tr><td>89.</td><td>301</td><td>65.250</td><td>-85.500</td><td>.000</td></tr> <tr><td>90.</td><td>302</td><td>65.250</td><td>-87.500</td><td>.000</td></tr> <tr><td>91.</td><td>303</td><td>65.250</td><td>-89.500</td><td>.000</td></tr> <tr><td>92.</td><td>304</td><td>65.250</td><td>-91.500</td><td>.000</td></tr> <tr><td>93.</td><td>305</td><td>65.250</td><td>-93.500</td><td>.000</td></tr> <tr><td>94.</td><td>306</td><td>65.250</td><td>-95.500</td><td>.000</td></tr> <tr><td>95.</td><td>307</td><td>65.250</td><td>-97.500</td><td>.000</td></tr> <tr><td>96.</td><td>308</td><td>65.250</td><td>-99.500</td><td>.000</td></tr> <tr><td>97.</td><td>309</td><td>65.250</td><td>-101.000</td><td>.000</td></tr> <tr><td>98.</td><td>310</td><td>74.750</td><td>-1.000</td><td>.000</td></tr> <tr><td>99.</td><td>311</td><td>74.750</td><td>-10.000</td><td>.000</td></tr> <tr><td>100.</td><td>312</td><td>74.750</td><td>-25.000</td><td>.000</td></tr> <tr><td>101.</td><td>313</td><td>74.750</td><td>-53.500</td><td>.000</td></tr> <tr><td>102.</td><td>314</td><td>74.750</td><td>-26.500</td><td>.000</td></tr> <tr><td>103.</td><td>315</td><td>74.750</td><td>-28.000</td><td>.000</td></tr> <tr><td>104.</td><td>316</td><td>74.750</td><td>-29.500</td><td>.000</td></tr> <tr><td>105.</td><td>317</td><td>74.750</td><td>-31.000</td><td>.000</td></tr> <tr><td>106.</td><td>318</td><td>74.750</td><td>-32.500</td><td>.000</td></tr> <tr><td>107.</td><td>319</td><td>74.750</td><td>-34.000</td><td>.000</td></tr> <tr><td>108.</td><td>320</td><td>74.750</td><td>-35.500</td><td>.000</td></tr> <tr><td>109.</td><td>321</td><td>74.750</td><td>-37.000</td><td>.000</td></tr> <tr><td>110.</td><td>322</td><td>74.750</td><td>-38.500</td><td>.000</td></tr> <tr><td>111.</td><td>323</td><td>74.750</td><td>-40.000</td><td>.000</td></tr> <tr><td>112.</td><td>324</td><td>74.750</td><td>-41.500</td><td>.000</td></tr> <tr><td>113.</td><td>325</td><td>74.750</td><td>-43.000</td><td>.000</td></tr> <tr><td>114.</td><td>326</td><td>74.750</td><td>-44.500</td><td>.000</td></tr> <tr><td>115.</td><td>327</td><td>74.750</td><td>-46.000</td><td>.000</td></tr> <tr><td>116.</td><td>328</td><td>74.750</td><td>-47.500</td><td>.000</td></tr> <tr><td>117.</td><td>329</td><td>74.750</td><td>-49.000</td><td>.000</td></tr> <tr><td>118.</td><td>330</td><td>74.750</td><td>-50.500</td><td>.000</td></tr> <tr><td>119.</td><td>331</td><td>74.750</td><td>-52.000</td><td>.000</td></tr> <tr><td>120.</td><td>332</td><td>74.750</td><td>-53.500</td><td>.000</td></tr> <tr><td>121.</td><td>333</td><td>74.750</td><td>-55.500</td><td>.000</td></tr> <tr><td>122.</td><td>334</td><td>74.750</td><td>-57.500</td><td>.000</td></tr> <tr><td>123.</td><td>335</td><td>74.750</td><td>-59.500</td><td>.000</td></tr> </table>	57.	269	65.250	-29.500	.000	58.	270	65.250	-31.000	.000	59.	271	65.250	-32.500	.000	60.	272	65.250	-34.000	.000	61.	273	65.250	-35.500	.000	62.	274	65.250	-37.000	.000	63.	275	65.250	-38.500	.000	64.	276	65.250	-40.000	.000	65.	277	65.250	-41.500	.000	66.	278	65.250	-43.000	.000	67.	279	65.250	-44.500	.000	68.	280	65.250	-46.000	.000	69.	281	65.250	-47.500	.000	70.	282	65.250	-49.000	.000	71.	283	65.250	-50.500	.000	72.	284	65.250	-52.000	.000	73.	285	65.250	-53.500	.000	74.	286	65.250	-55.000	.000	75.	287	65.250	-57.500	.000	76.	288	65.250	-59.500	.000	77.	289	65.250	-61.500	.000	78.	290	65.250	-63.500	.000	79.	291	65.250	-65.500	.000	80.	292	65.250	-67.500	.000	81.	293	65.250	-69.500	.000	82.	294	65.250	-71.500	.000	83.	295	65.250	-73.500	.000	84.	296	65.250	-75.500	.000	85.	297	65.250	-77.500	.000	86.	298	65.250	-79.500	.000	87.	299	65.250	-81.500	.000	88.	300	65.250	-83.500	.000	89.	301	65.250	-85.500	.000	90.	302	65.250	-87.500	.000	91.	303	65.250	-89.500	.000	92.	304	65.250	-91.500	.000	93.	305	65.250	-93.500	.000	94.	306	65.250	-95.500	.000	95.	307	65.250	-97.500	.000	96.	308	65.250	-99.500	.000	97.	309	65.250	-101.000	.000	98.	310	74.750	-1.000	.000	99.	311	74.750	-10.000	.000	100.	312	74.750	-25.000	.000	101.	313	74.750	-53.500	.000	102.	314	74.750	-26.500	.000	103.	315	74.750	-28.000	.000	104.	316	74.750	-29.500	.000	105.	317	74.750	-31.000	.000	106.	318	74.750	-32.500	.000	107.	319	74.750	-34.000	.000	108.	320	74.750	-35.500	.000	109.	321	74.750	-37.000	.000	110.	322	74.750	-38.500	.000	111.	323	74.750	-40.000	.000	112.	324	74.750	-41.500	.000	113.	325	74.750	-43.000	.000	114.	326	74.750	-44.500	.000	115.	327	74.750	-46.000	.000	116.	328	74.750	-47.500	.000	117.	329	74.750	-49.000	.000	118.	330	74.750	-50.500	.000	119.	331	74.750	-52.000	.000	120.	332	74.750	-53.500	.000	121.	333	74.750	-55.500	.000	122.	334	74.750	-57.500	.000	123.	335	74.750	-59.500	.000	<table border="0" style="width: 100%; border-collapse: collapse;"> <tr><td>124.</td><td>336</td><td>74.750</td><td>-61.500</td><td>.000</td></tr> <tr><td>125.</td><td>337</td><td>74.750</td><td>-63.500</td><td>.000</td></tr> <tr><td>126.</td><td>338</td><td>74.750</td><td>-65.500</td><td>.000</td></tr> <tr><td>127.</td><td>339</td><td>74.750</td><td>-67.500</td><td>.000</td></tr> <tr><td>128.</td><td>340</td><td>74.750</td><td>-69.500</td><td>.000</td></tr> <tr><td>129.</td><td>341</td><td>74.750</td><td>-71.500</td><td>.000</td></tr> <tr><td>130.</td><td>342</td><td>74.750</td><td>-73.500</td><td>.000</td></tr> <tr><td>131.</td><td>343</td><td>74.750</td><td>-75.500</td><td>.000</td></tr> <tr><td>132.</td><td>344</td><td>74.750</td><td>-77.500</td><td>.000</td></tr> <tr><td>133.</td><td>345</td><td>74.750</td><td>-79.500</td><td>.000</td></tr> <tr><td>134.</td><td>346</td><td>74.750</td><td>-81.500</td><td>.000</td></tr> <tr><td>135.</td><td>347</td><td>74.750</td><td>-83.500</td><td>.000</td></tr> <tr><td>136.</td><td>348</td><td>74.750</td><td>-85.500</td><td>.000</td></tr> <tr><td>137.</td><td>349</td><td>74.750</td><td>-87.500</td><td>.000</td></tr> <tr><td>138.</td><td>350</td><td>74.750</td><td>-89.500</td><td>.000</td></tr> <tr><td>139.</td><td>351</td><td>74.750</td><td>-91.500</td><td>.000</td></tr> <tr><td>140.</td><td>352</td><td>74.750</td><td>-93.500</td><td>.000</td></tr> <tr><td>141.</td><td>353</td><td>74.750</td><td>-95.500</td><td>.000</td></tr> <tr><td>142.</td><td>354</td><td>74.750</td><td>-97.500</td><td>.000</td></tr> <tr><td>143.</td><td>355</td><td>74.750</td><td>-101.000</td><td>.000</td></tr> </table> <p>144. MEMBER INCIDENCES</p> <table border="0" style="width: 100%; border-collapse: collapse;"> <tr><td>145.</td><td>1</td><td>1</td><td>2</td></tr> <tr><td>146.</td><td>2</td><td>2</td><td>3</td></tr> <tr><td>147.</td><td>3</td><td>3</td><td>4</td></tr> <tr><td>148.</td><td>4</td><td>4</td><td>5</td></tr> <tr><td>149.</td><td>5</td><td>5</td><td>6</td></tr> <tr><td>150.</td><td>6</td><td>6</td><td>7</td></tr> <tr><td>151.</td><td>7</td><td>7</td><td>8</td></tr> <tr><td>152.</td><td>8</td><td>8</td><td>9</td></tr> <tr><td>153.</td><td>9</td><td>9</td><td>10</td></tr> <tr><td>154.</td><td>10</td><td>10</td><td>11</td></tr> <tr><td>155.</td><td>11</td><td>11</td><td>12</td></tr> <tr><td>156.</td><td>12</td><td>12</td><td>13</td></tr> <tr><td>157.</td><td>13</td><td>13</td><td>14</td></tr> <tr><td>158.</td><td>14</td><td>14</td><td>15</td></tr> <tr><td>159.</td><td>15</td><td>15</td><td>16</td></tr> <tr><td>160.</td><td>16</td><td>16</td><td>17</td></tr> <tr><td>161.</td><td>17</td><td>17</td><td>18</td></tr> <tr><td>162.</td><td>18</td><td>18</td><td>19</td></tr> <tr><td>163.</td><td>19</td><td>19</td><td>20</td></tr> <tr><td>164.</td><td>20</td><td>20</td><td>21</td></tr> <tr><td>165.</td><td>21</td><td>21</td><td>22</td></tr> <tr><td>166.</td><td>22</td><td>22</td><td>23</td></tr> <tr><td>167.</td><td>23</td><td>23</td><td>24</td></tr> <tr><td>168.</td><td>24</td><td>24</td><td>25</td></tr> <tr><td>169.</td><td>25</td><td>25</td><td>26</td></tr> <tr><td>170.</td><td>26</td><td>26</td><td>27</td></tr> <tr><td>171.</td><td>27</td><td>27</td><td>28</td></tr> <tr><td>172.</td><td>28</td><td>28</td><td>29</td></tr> <tr><td>173.</td><td>29</td><td>29</td><td>30</td></tr> <tr><td>174.</td><td>30</td><td>30</td><td>31</td></tr> <tr><td>175.</td><td>31</td><td>31</td><td>32</td></tr> <tr><td>176.</td><td>32</td><td>32</td><td>33</td></tr> <tr><td>177.</td><td>33</td><td>33</td><td>34</td></tr> <tr><td>178.</td><td>34</td><td>34</td><td>35</td></tr> <tr><td>179.</td><td>35</td><td>35</td><td>36</td></tr> <tr><td>180.</td><td>36</td><td>36</td><td>37</td></tr> <tr><td>181.</td><td>203</td><td>22</td><td>204</td></tr> <tr><td>182.</td><td>204</td><td>204</td><td>205</td></tr> <tr><td>183.</td><td>205</td><td>205</td><td>206</td></tr> <tr><td>184.</td><td>206</td><td>206</td><td>207</td></tr> <tr><td>185.</td><td>207</td><td>207</td><td>208</td></tr> <tr><td>186.</td><td>208</td><td>208</td><td>209</td></tr> <tr><td>187.</td><td>239</td><td>209</td><td>240</td></tr> <tr><td>188.</td><td>240</td><td>240</td><td>241</td></tr> <tr><td>189.</td><td>241</td><td>209</td><td>242</td></tr> <tr><td>190.</td><td>242</td><td>242</td><td>243</td></tr> </table>	124.	336	74.750	-61.500	.000	125.	337	74.750	-63.500	.000	126.	338	74.750	-65.500	.000	127.	339	74.750	-67.500	.000	128.	340	74.750	-69.500	.000	129.	341	74.750	-71.500	.000	130.	342	74.750	-73.500	.000	131.	343	74.750	-75.500	.000	132.	344	74.750	-77.500	.000	133.	345	74.750	-79.500	.000	134.	346	74.750	-81.500	.000	135.	347	74.750	-83.500	.000	136.	348	74.750	-85.500	.000	137.	349	74.750	-87.500	.000	138.	350	74.750	-89.500	.000	139.	351	74.750	-91.500	.000	140.	352	74.750	-93.500	.000	141.	353	74.750	-95.500	.000	142.	354	74.750	-97.500	.000	143.	355	74.750	-101.000	.000	145.	1	1	2	146.	2	2	3	147.	3	3	4	148.	4	4	5	149.	5	5	6	150.	6	6	7	151.	7	7	8	152.	8	8	9	153.	9	9	10	154.	10	10	11	155.	11	11	12	156.	12	12	13	157.	13	13	14	158.	14	14	15	159.	15	15	16	160.	16	16	17	161.	17	17	18	162.	18	18	19	163.	19	19	20	164.	20	20	21	165.	21	21	22	166.	22	22	23	167.	23	23	24	168.	24	24	25	169.	25	25	26	170.	26	26	27	171.	27	27	28	172.	28	28	29	173.	29	29	30	174.	30	30	31	175.	31	31	32	176.	32	32	33	177.	33	33	34	178.	34	34	35	179.	35	35	36	180.	36	36	37	181.	203	22	204	182.	204	204	205	183.	205	205	206	184.	206	206	207	185.	207	207	208	186.	208	208	209	187.	239	209	240	188.	240	240	241	189.	241	209	242	190.	242	242	243
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THE STUDY ON THE CONSTRUCTION OF THE BRIDGE OVER THE RIVER RUPSA IN KHULNA, PHASE - 2

Job No. :	Designed by :	Checked by :	Date : 1/24/00
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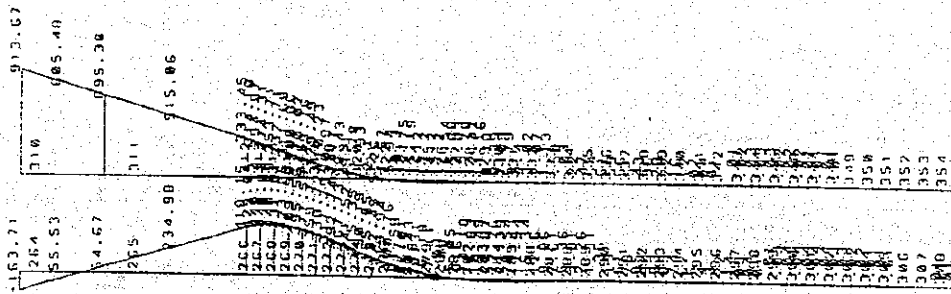
191.	263	240	264	258.	330	331	313
192.	264	264	265	259.	331	313	333
193.	265	265	266	260.	332	333	334
194.	266	266	267	261.	333	334	335
195.	267	267	268	262.	334	335	336
196.	268	268	269	263.	335	336	337
197.	269	269	270	264.	336	337	338
198.	270	270	271	265.	337	338	339
199.	271	271	272	266.	338	339	340
200.	272	272	273	267.	339	340	341
201.	273	273	274	268.	340	341	342
202.	274	274	275	269.	341	342	343
203.	275	275	276	270.	342	343	344
204.	276	276	277	271.	343	344	345
205.	277	277	278	272.	344	345	346
206.	278	278	279	273.	345	346	347
207.	279	279	280	274.	346	347	348
208.	280	280	281	275.	347	348	349
209.	281	281	282	276.	348	349	350
210.	282	282	283	277.	349	350	351
211.	283	283	284	278.	350	351	352
212.	284	284	285	279.	351	352	353
213.	285	285	286	280.	352	353	354
214.	286	286	287	281.	353	354	355
215.	287	287	288	282.	354	355	
216.	288	288	289	283.	MEMBER PROPERTY AMERICAN		
217.	289	289	290	284.	1 TO 12 31 TO 36 PRJ AX 11.661 IX 227.335 IY 211.669 IZ 15.686 YD 3.12		
218.	290	290	291	285.	13 30 PRJ AX 11.976 IX 232.635 IY 214.3 IZ 18.344 YD 3.45		
219.	291	291	292	286.	14 29 PRJ AX 12.595 IX 243.458 IY 219.291 IZ 24.092 YD 3.79		
220.	292	292	293	287.	15 28 PRJ AX 13.192 IX 254.458 IY 223.749 IZ 30.71 YD 4.13		
221.	293	293	294	288.	16 27 PRJ AX 13.767 IX 265.913 IY 227.707 IZ 38.206 YD 4.46		
222.	294	294	295	289.	17 26 PRJ AX 14.319 IX 277.778 IY 231.196 IZ 46.582 YD 4.8		
223.	295	295	296	290.	18 25 PRJ AX 14.849 IX 290.081 IY 234.246 IZ 55.835 YD 5.13		
224.	296	296	297	291.	19 24 PRJ AX 15.357 IX 302.84 IY 236.885 IZ 65.955 YD 5.47		
225.	297	297	298	292.	20 23 PRJ AX 15.919 IX 318.365 IY 239.447 IZ 78.918 YD 5.92		
226.	298	298	299	293.	21 22 PRJ AX 16.232 IX 327.395 IY 240.789 IZ 86.606 YD 5.92		
227.	299	299	300	294.	203 204 207 208 PRJ AX 16.5 IX 92.71 IY 29.69 IZ 63.02		
228.	300	300	301	295.	205 206 PRJ AX 14.5 IX 72.7 IY 24.02 IZ 48.68		
229.	301	301	302	296.	239 TO 242 263 TO 354 PRJ AX 14.73 IX 11.505 IY 5.752 IZ 5.752		
230.	302	302	303	297.	CONSTANT		
231.	303	303	304	298.	E 2.500E7 ALL		
232.	304	304	305	299.	POISSON CONCRETE ALL		
233.	305	305	306	300.	ALPHA .0000108 MEMB 1 TO 36		
234.	306	306	307	301.	DENSITY 24. MEMB 1 TO 36 203 TO 208 239 TO 242		
235.	307	307	308	302.	DENSITY 14.2 MEMB 239 TO 242 263 TO 354		
236.	308	308	309	303.	SUPPORT		
237.	309	242	310	304.	1 FIXED BUT FX MX MY MZ		
238.	310	310	311	305.	266 312 FIXED BUT FY MX MY MZ KFX 4.370E4 KFZ 4.370E4		
239.	311	311	312	306.	267 TO 270 314 TO 317 FIXED BUT FY MX MY MZ KFX 8.750E4 KFZ 8.750E4		
240.	312	312	314	307.	271 318 FIXED BUT FY MX MY MZ KFX 112840. KFZ 112840.		
241.	313	314	315	308.	272 273 319 320 FIXED BUT FY MX MY MZ KFX 3.030E5 KFZ 3.030E5		
242.	314	315	316	309.	274 TO 281 321 TO 328 FIXED BUT FY MX MY MZ KFX 3.930E5 KFZ 3.930E5		
243.	315	316	317	310.	282 283 329 330 FIXED BUT FY MX MY MZ KFX 5.237E5 KFZ 5.237E5		
244.	316	317	318	311.	284 TO 288 313 331 333 TO 334 -		
245.	317	318	319	312.	335 FIXED BUT FY MX MY MZ KFX 1.560E6 KFZ 1.560E6		
246.	318	319	320	313.	289 TO 308 332 336 TO 354 FIXED BUT FY MX MY MZ KFX 1.426E6 KFZ 1.426E6		
247.	319	320	321				
248.	320	321	322				
249.	321	322	323				
250.	322	323	324				
251.	323	324	325				
252.	324	325	326				
253.	325	326	327				
254.	326	327	328				
255.	327	328	329				
256.	328	329	330				
257.	329	330	331				

DEAD LOAD ANALYSIS
OF FRAME 2

11/11/2010

11/11/2010

MN/ELEM
MOMENT MZ L.N= 1



Maximum= 913.67
J=139, H=138

UNIT MET KNS

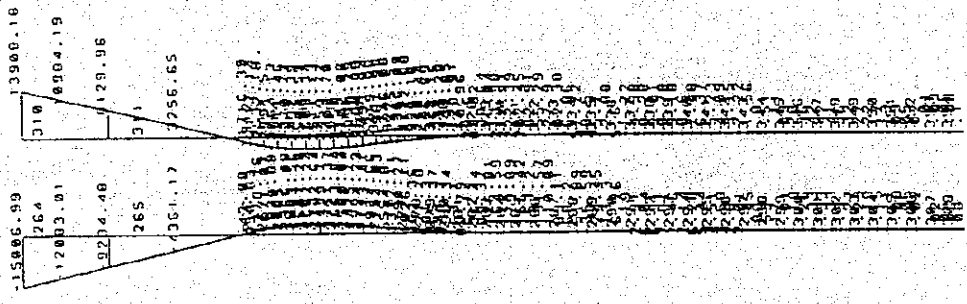
S T A A D P O S T - P L O T (REV: 22.3)
TITLE: RUPSA BRIDGE IN KIHUNA

DATE: JAN 24, 2000

STRUCTURE DATA

TYPE = SPACE
NJ = 139
NM = 138
NE = 0
NS = 0
NRJ = 89
NL = 2
XMAX = 118.0
YMAX = 121.3
ZMAX = .0

MN/ELEM
MOMENT MZ LN= 2



STRUCTURE DATA

TYPE = SPACE
 NJ = 139
 NH = 138
 NE = 0
 NS = 0
 NRJ = 89
 NL = 2
 XMAX = 118.0
 YMAX = 121.3
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Maximum = 15006.99
 U=139, M=138

UNIT MET KNS

S T A A D P O S T - P L O T (REV: 22.3)

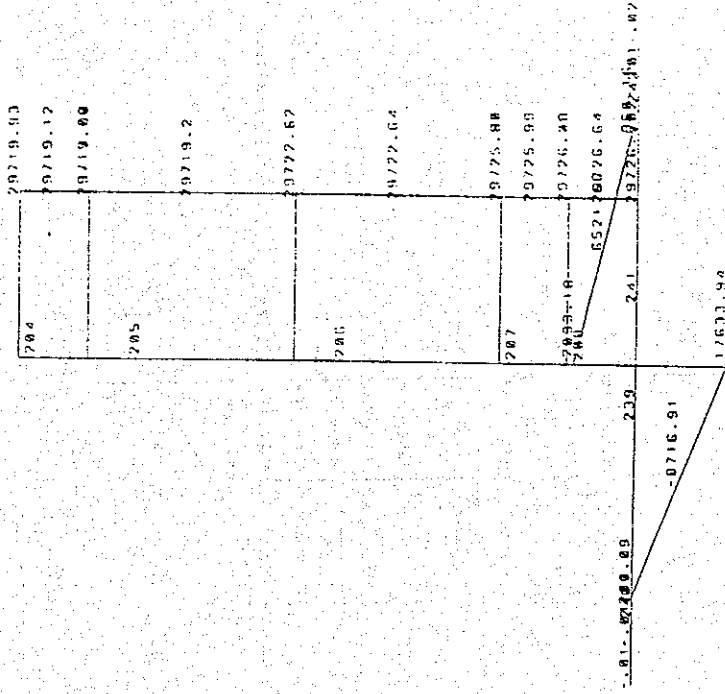
TITLE: RUPSA BRIDGE IN KHULNA

DATE: JAN 24, 2000

MN/ELEM
MOMINI M7 LN: 1

STRUCTURE DATA

TYPE = SPACE
 NJ = 139
 NM = 130
 NE = 0
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 NL = 2
 XMAX = 110.0
 YMAX = 121.3
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Maximum: 29726.77
 J=139.M.130

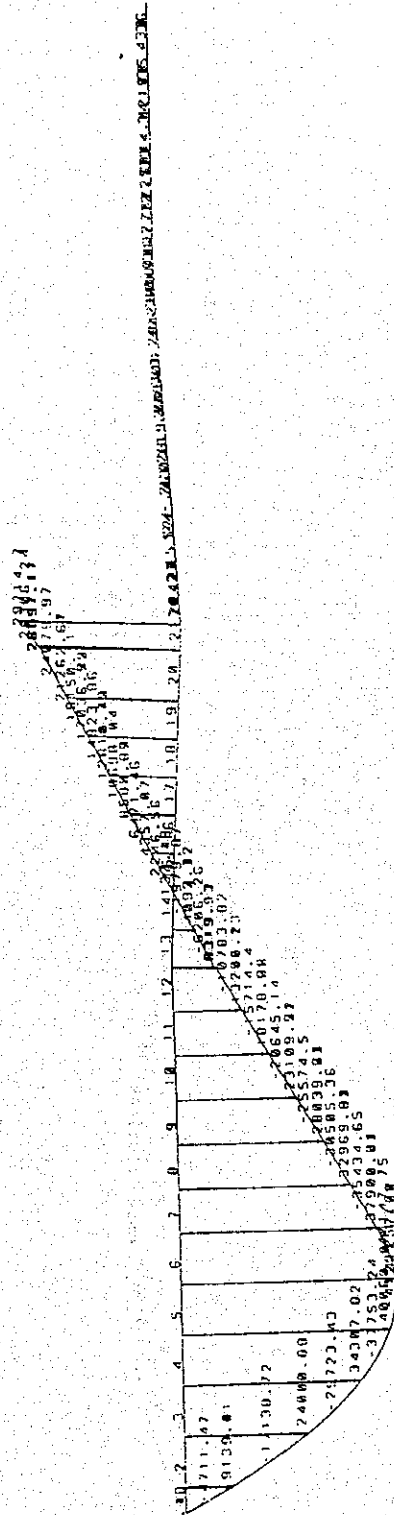
UNIT: MET KNS

DATE: JAN 24, 2000

S I A A D P O S I P L O I (REV: 22.3)
 TITLE: RUPSA BRIDGE IN KHULNA

MN/ELEM
 N0MINI M7 LNE 1

STRUCTURE DATA
 TYPE = SPACE
 NJ = 139
 NH = 138
 NE = 0
 NS = 0
 NRJ = 89
 NL = 2
 XMAX = 118.0
 YMAX = 121.3
 ZMAX = .0



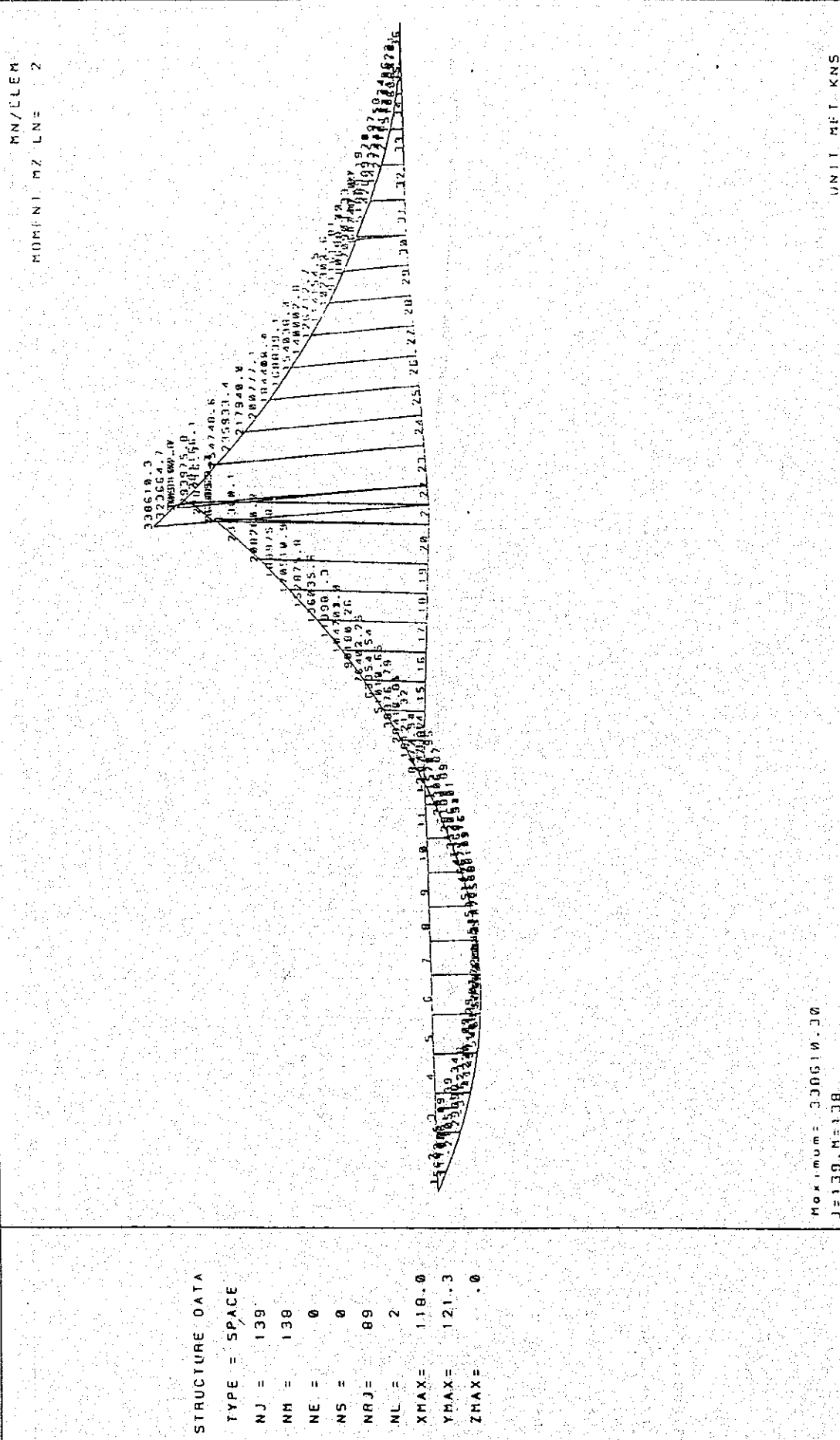
Maximum: 41384.52
 J=139, M=138

UNIT MET KNS

DATE: JAN 24, 2000

S I A A D P O S I P L O I (REV: 22.3)

TITLE: RUPSA BRIDGE IN KHULNA



THE STUDY ON THE CONSTRUCTION OF THE BRIDGE OVER THE RIVER RUPSA IN KHULNA, PHASE - 2

Job No. :

Designed by :

Checked by :

Date :

January 24, 2000

LOADS

LOAD 1 SELFWEIGHT OF CLOSURE SPAN ALONE

MEMBER LOAD
1 TO 6 UNIGY -284.53

LOAD 2 SELFWEIGHT OF WHOLE STRUCTURE

SELFWEIGHT Y -I

THE STUDY ON THE CONSTRUCTION OF THE BRIDGE OVER THE RIVER RUPSA IN KHULNA, PHASE - 2

Job No. : Designed by : Checked by : Date : January 24, 2000

DEAD LOAD ANALYSIS

MEMB	LOAD	NODE	AXIAL	SHEAR-Y	SHEAR-Z	TORSION	MON-Y	MOM-Z
1	1	1	138.22	4,852.00	0.00	0.00	0.00	0.18
		2	-122.00	-1,282.94	0.00	0.00	0.00	9,139.01
	2	1	165.97	5,820.13	0.00	0.00	0.00	0.00
		2	-150.02	-5,260.40	0.00	0.00	0.00	11,086.09
2	1	2	128.54	4,282.53	0.00	0.00	0.00	-9,139.11
		3	-94.40	-3,144.41	0.00	0.00	0.00	24,000.69
	2	2	157.84	5,260.25	0.00	0.00	0.00	-11,086.13
		3	-124.26	-4,140.80	0.00	0.00	0.00	29,898.30
3	1	3	94.32	3,144.52	0.00	0.00	0.00	-24,000.88
		4	-60.18	-2,006.40	0.00	0.00	0.00	34,307.92
	2	3	124.18	4,140.86	0.00	0.00	0.00	-29,898.34
		4	-90.60	-3,021.40	0.00	0.00	0.00	44,230.09
4	1	4	60.25	2,006.46	0.00	0.00	0.00	-34,307.82
		5	-26.11	-868.34	0.00	0.00	0.00	40,060.04
	2	4	90.71	3,021.09	0.00	0.00	0.00	-44,230.37
		5	-57.12	-1,901.63	0.00	0.00	0.00	54,081.51
5	1	5	26.05	868.10	0.00	0.00	0.00	-40,060.00
		6	8.09	270.02	0.00	0.00	0.00	41,257.09
	2	5	57.09	1,901.96	0.00	0.00	0.00	-54,081.07
		6	-23.51	-782.51	0.00	0.00	0.00	59,453.58
6	1	6	-8.12	-269.96	0.00	0.00	0.00	-41,257.24
		7	42.26	1,408.08	0.00	0.00	0.00	37,900.41
	2	6	23.64	782.50	0.00	0.00	0.00	-59,452.12
		7	9.94	336.95	0.00	0.00	0.00	60,344.87
7	1	7	-42.26	-1,408.44	0.00	0.00	0.00	-37,900.53
		8	42.26	1,408.44	0.00	0.00	0.00	32,969.31
	2	7	-10.14	-537.15	0.00	0.00	0.00	-60,343.27
		8	39.52	1,316.68	0.00	0.00	0.00	57,449.56
8	1	8	-42.20	-1,407.53	0.00	0.00	0.00	-32,969.63
		9	42.20	1,407.53	0.00	0.00	0.00	28,039.63
	2	8	-39.27	-1,316.06	0.00	0.00	0.00	-57,448.68
		9	68.66	2,295.58	0.00	0.00	0.00	51,125.19
9	1	9	-42.24	-1,408.06	0.00	0.00	0.00	-28,039.71
		10	42.24	1,408.06	0.00	0.00	0.00	23,109.52
	2	9	-68.98	-2,296.74	0.00	0.00	0.00	-51,125.01
		10	98.37	3,276.27	0.00	0.00	0.00	41,368.94
10	1	10	-42.29	-1,407.69	0.00	0.00	0.00	-23,109.71
		11	42.29	1,407.69	0.00	0.00	0.00	18,179.28
	2	10	-98.37	-3,275.80	0.00	0.00	0.00	-41,369.97
		11	127.76	4,255.32	0.00	0.00	0.00	28,184.89
11	1	11	-42.22	-1,407.49	0.00	0.00	0.00	-18,178.62
		12	42.22	1,407.49	0.00	0.00	0.00	13,250.20
	2	11	-127.89	-4,255.25	0.00	0.00	0.00	-28,185.63
		12	157.28	5,234.77	0.00	0.00	0.00	11,570.95
12	1	12	-42.23	-1,408.11	0.00	0.00	0.00	-13,249.13
		13	42.23	1,408.11	0.00	0.00	0.00	8,319.12
	2	12	-156.92	-5,234.65	0.00	0.00	0.00	-11,571.30
		13	186.30	6,214.17	0.00	0.00	0.00	-8,472.31
13	1	13	35.65	-1,408.66	0.00	0.00	0.00	-8,319.93
		14	-35.65	1,408.66	0.00	0.00	0.00	4,092.32
	2	13	157.52	-6,214.89	0.00	0.00	0.00	8,472.54
		14	-179.37	7,077.16	0.00	0.00	0.00	-28,417.91
14	1	14	36.64	-1,408.34	0.00	0.00	0.00	-4,093.10
		15	-36.64	1,408.34	0.00	0.00	0.00	-133.18

THE STUDY ON THE CONSTRUCTION OF THE BRIDGE OVER THE RIVER RUPSA IN KHULNA, PHASE - 2

Job No. :	Designed by :	Checked by :	Date :	January 24, 2000				
	2	14	183.71	-7,077.44	0.00	0.00	0.00	28,416.85
		15	-207.29	7,984.28	0.00	0.00	0.00	-51,018.65
15	1	15	37.05	-1,407.59	0.00	0.00	0.00	134.45
		16	-37.05	1,407.59	0.00	0.00	0.00	-4,357.67
	2	15	210.28	-7,984.33	0.00	0.60	0.00	51,017.60
		16	-255.29	8,934.15	0.00	0.00	0.00	-76,403.25
16	1	16	37.78	-1,408.83	0.00	0.00	0.00	4,357.47
		17	-37.78	1,408.83	0.00	0.00	0.00	-8,585.89
	2	16	238.08	-8,933.94	0.00	0.00	0.00	76,402.75
		17	-264.51	9,925.17	0.00	0.00	0.00	-104,703.40
17	1	17	37.44	-1,408.41	0.00	0.00	0.00	8,584.67
		18	-37.44	1,408.41	0.00	0.00	0.00	-12,810.79
	2	17	264.43	-9,925.28	0.00	0.00	0.00	104,701.30
		18	-291.93	10,936.25	0.00	0.00	0.00	-136,035.60
18	1	18	37.46	-1,407.79	0.00	0.00	0.00	12,811.42
		19	-37.46	1,407.79	0.00	0.00	0.00	-17,036.92
	2	18	292.07	-10,935.58	0.00	0.00	0.00	136,035.50
		19	-320.58	12,024.71	0.00	0.00	0.00	-170,517.50
19	1	19	36.69	-1,408.43	0.00	0.00	0.00	17,036.73
		20	-36.69	1,408.43	0.00	0.00	0.00	-21,262.61
	2	19	312.70	-12,024.42	0.00	0.00	0.00	170,518.30
		20	-341.45	13,130.13	0.00	0.00	0.00	-208,265.20
20	1	20	35.45	-1,408.20	0.00	0.00	0.00	21,262.67
		21	-35.45	1,408.20	0.00	0.00	0.00	-26,897.17
	2	20	331.72	-13,131.46	0.00	0.00	0.00	208,264.50
		21	-370.31	14,659.68	0.00	0.00	0.00	-263,863.10
21	1	21	17.66	-1,408.96	0.00	0.00	0.00	26,897.17
		22	-17.66	1,408.96	0.00	0.00	0.00	-29,714.70
	2	21	183.04	-14,665.05	0.00	0.00	0.00	263,855.70
		22	-192.78	15,444.19	0.00	0.00	0.00	-293,975.80
22	1	22	-0.17	0.31	0.00	0.00	0.00	0.36
		23	0.17	-0.31	0.00	0.00	0.00	0.26
	2	22	1,094.93	15,101.25	0.00	0.00	0.00	338,610.30
		23	-1,038.44	-14,322.11	0.00	0.00	0.00	-309,102.70
23	1	23	0.07	-0.07	0.00	0.00	0.00	0.00
		24	-0.07	0.07	0.00	0.00	0.00	-0.12
	2	23	1,219.69	14,303.88	0.00	0.00	0.00	309,110.80
		24	-1,089.40	-12,775.66	0.00	0.00	0.00	-254,748.30
24	1	24	-0.19	0.07	0.00	0.00	0.00	0.30
		25	0.19	-0.07	0.00	0.00	0.00	0.43
	2	24	1,098.46	12,773.78	0.00	0.00	0.00	254,748.60
		25	-1,003.37	-11,668.08	0.00	0.00	0.00	-217,943.30
25	1	25	0.04	-0.14	0.00	0.00	0.00	-0.23
		26	-0.04	0.14	0.00	0.00	0.00	-0.23
	2	25	1,011.03	11,666.76	0.00	0.00	0.00	217,940.40
		26	-918.37	-10,397.64	0.00	0.00	0.00	-184,412.40
26	1	26	0.12	0.05	0.00	0.00	0.00	0.26
		27	-0.12	-0.05	0.00	0.00	0.00	0.06
	2	26	918.34	10,598.31	0.00	0.00	0.00	184,408.10
		27	-828.99	-9,567.35	0.00	0.00	0.00	-154,038.40
27	1	27	0.07	0.16	0.00	0.00	0.00	-0.71
		28	-0.07	-0.16	0.00	0.00	0.00	-0.34
	2	27	829.28	9,568.59	0.00	0.00	0.00	154,036.30
		28	-743.37	-8,577.36	0.00	0.00	0.00	-126,712.70
28	1	28	0.38	-0.23	0.00	0.00	0.00	-0.06
		29	-0.38	0.23	0.00	0.00	0.00	-0.28
	2	28	740.43	8,578.10	0.00	0.00	0.00	126,712.00
		29	-658.43	-7,628.28	0.00	0.00	0.00	-102,303.60
29	1	29	-0.02	0.06	0.00	0.00	0.00	-0.11
		30	0.02	-0.06	0.00	0.00	0.00	-0.23

THE STUDY ON THE CONSTRUCTION OF THE BRIDGE OVER THE RIVER RUPSA IN KHULNA, PHASE - 2

Job No. :	Designed by :	Checked by :	Date :	January 24, 2000				
	2	29	655.73	7,626.20	0.00	0.00	0.00	102,302.00
		30	-577.74	-6,719.36	0.00	0.00	0.00	-80,699.42
30	1	30	-0.26	-0.03	0.00	0.00	0.00	0.34
		31	0.26	0.03	0.00	0.00	0.00	0.23
	2	30	573.67	6,721.87	0.00	0.00	0.00	80,698.25
		31	-500.09	-5,859.59	0.00	0.00	0.00	-61,748.09
31	1	31	-0.01	0.18	0.00	0.00	0.00	0.04
		32	0.01	-0.18	0.00	0.00	0.00	0.37
	2	31	176.35	5,875.94	0.00	0.00	0.00	61,747.97
		32	-146.97	-4,896.42	0.60	0.00	0.00	-42,885.71
32	1	32	0.01	0.08	0.00	0.00	0.00	-0.11
		33	-0.01	-0.08	0.00	0.00	0.00	0.17
	2	32	146.99	4,898.67	0.00	0.00	0.00	42,885.78
		33	-117.61	-3,919.14	0.00	0.00	0.00	-27,443.59
33	1	33	-0.04	0.16	0.00	0.00	0.00	0.73
		34	0.04	-0.16	0.00	0.00	0.00	0.42
	2	33	117.54	3,916.28	0.00	0.00	0.00	27,442.18
		34	-88.16	-2,936.76	0.00	0.00	0.00	-15,441.50
34	1	34	-0.03	-0.23	0.00	0.00	0.00	-0.57
		35	0.03	0.23	0.00	0.00	0.00	-0.88
	2	34	88.22	2,937.86	0.00	0.00	0.00	15,434.06
		35	-58.84	-1,958.34	0.00	0.00	0.00	-6,865.40
35	1	35	0.10	0.24	0.00	0.00	0.00	0.01
		36	-0.10	-0.24	0.00	0.00	0.00	-0.34
	2	35	58.80	1,959.87	0.00	0.00	0.00	6,862.01
		36	-29.41	-980.34	0.00	0.00	0.00	-1,717.33
36	1	36	-0.04	0.52	0.00	0.00	0.00	0.32
		37	0.04	-0.52	0.00	0.00	0.00	0.45
	2	36	29.30	979.98	0.00	0.00	0.00	1,713.94
		37	0.09	-0.45	0.00	0.00	0.00	-0.67
204	1	204	1,408.63	1.08	0.00	0.00	0.00	29,719.93
		205	-1,408.63	-1.08	0.00	0.00	0.00	-29,717.08
	2	204	31,925.87	5.04	0.00	0.00	0.00	-44,740.99
		205	-32,519.87	-5.04	0.00	0.00	0.00	44,789.42
205	1	205	1,408.63	-0.06	0.00	0.00	0.00	29,719.07
		206	-1,408.63	0.06	0.00	0.00	0.00	-29,722.52
	2	205	32,519.97	-0.16	0.00	0.00	0.00	-44,787.42
		206	-34,085.97	0.16	0.00	0.00	0.00	44,910.94
206	1	206	1,408.63	-0.01	0.00	0.00	0.00	29,722.62
		207	-1,408.63	0.01	0.00	0.00	0.00	-29,725.54
	2	206	34,086.15	-0.08	0.00	0.00	0.00	-44,911.26
		207	-35,652.15	0.08	0.00	0.00	0.00	45,016.23
207	1	207	1,408.62	-0.13	0.00	0.00	0.00	29,725.89
		208	-1,408.62	0.13	0.00	0.00	0.00	-29,726.28
	2	207	35,652.10	0.07	0.00	0.00	0.00	-45,014.68
		208	-36,246.10	-0.07	0.00	0.00	0.00	45,048.75
239	1	209	24.05	-3,754.54	0.00	0.00	0.00	-17,633.94
		240	-24.05	3,754.54	0.00	0.00	0.00	-200.09
	2	209	649.79	-13,791.08	0.00	0.00	0.00	-51,591.96
		240	-649.79	14,784.62	0.00	0.00	0.00	-16,278.47
240	1	240	0.00	0.03	0.00	0.00	0.00	0.02
		241	0.00	-0.03	0.00	0.00	0.00	0.01
	2	240	0.05	366.07	0.00	0.00	0.00	320.29
		241	-0.05	-0.03	0.00	0.00	0.00	0.08
241	1	209	24.03	2,345.90	0.00	0.00	0.00	12,093.18
		242	-24.03	-2,345.90	0.00	0.00	0.00	-950.16
	2	209	649.76	-23,048.89	0.00	0.00	0.00	-96,668.27
		242	-649.76	24,042.43	0.00	0.00	0.00	-15,175.13
242	1	242	0.00	0.03	0.00	0.00	0.00	0.02
		243	0.00	-0.03	0.00	0.00	0.00	0.02

THE STUDY ON THE CONSTRUCTION OF THE BRIDGE OVER THE RIVER RUPSA IN KHULNA, PHASE - 2

Job No. :	Designed by :	Checked by :	Date :	January 24, 2000				
	2	242	-0.01	365.72	0.00	0.00	0.00	319.71
		243	0.01	0.32	0.00	0.00	0.00	-0.53
263	1	240	3,754.55	-24.05	0.00	0.00	0.00	-200.06
		264	-3,754.55	24.05	0.00	0.00	0.00	163.71
	2	240	15,150.64	-649.73	0.00	0.00	0.00	-15,958.14
		264	-15,464.39	649.73	0.00	0.00	0.00	15,007.06
264	1	264	3,754.54	-24.04	0.00	0.00	0.00	-163.71
		265	-3,754.54	24.04	0.00	0.00	0.00	-54.67
	2	264	15,464.45	-649.77	0.00	0.00	0.00	-15,006.99
		265	-17,346.94	649.77	0.00	0.00	0.00	9,234.48
265	1	265	3,754.53	-24.04	0.00	0.00	0.00	54.67
		266	-3,754.53	24.04	0.00	0.00	0.00	-418.15
	2	265	17,346.91	-649.77	0.00	0.00	0.00	-9,234.48
		266	-20,484.39	649.77	0.00	0.00	0.00	-542.80
266	1	266	3,754.54	-14.54	0.00	0.00	0.00	418.15
		267	-3,754.54	14.54	0.00	0.00	0.00	-440.14
	2	266	20,484.43	-552.06	0.00	0.00	0.00	542.78
		267	-20,798.18	552.06	0.00	0.00	0.00	-1,378.75
267	1	267	3,754.54	0.14	0.00	0.00	0.00	440.14
		268	-3,754.54	-0.14	0.00	0.00	0.00	-440.09
	2	267	20,798.24	-389.75	0.00	0.00	0.00	1,378.76
		268	-21,111.98	389.75	0.00	0.00	0.00	-1,970.95
268	1	268	3,754.54	11.05	0.00	0.00	0.00	440.10
		269	-3,754.54	-11.05	0.00	0.00	0.00	-423.65
	2	268	21,111.92	-258.83	0.00	0.00	0.00	1,970.88
		269	-21,425.66	258.83	0.00	0.00	0.00	-2,366.10
269	1	269	3,754.55	18.80	0.00	0.00	0.00	423.65
		270	-3,754.55	-18.80	0.00	0.00	0.00	-395.56
	2	269	21,425.64	-156.64	0.00	0.00	0.00	2,366.08
		270	-21,739.39	156.64	0.00	0.00	0.00	-2,607.28
270	1	270	3,754.52	23.97	0.00	0.00	0.00	395.56
		271	-3,754.52	-23.97	0.00	0.00	0.00	-359.70
	2	270	21,739.39	-79.95	0.00	0.00	0.00	2,607.27
		271	-22,053.14	79.95	0.00	0.00	0.00	-2,732.63
271	1	271	3,754.53	27.99	0.00	0.00	0.00	359.70
		272	-3,754.53	-27.99	0.00	0.00	0.00	-317.79
	2	271	22,053.18	-9.40	0.00	0.00	0.00	2,732.64
		272	-22,366.92	9.40	0.00	0.00	0.00	-2,731.37
272	1	272	3,754.54	33.40	0.00	0.00	0.00	317.79
		273	-3,754.54	-33.40	0.00	0.00	0.00	-267.73
	2	272	22,366.87	116.70	0.00	0.00	0.00	2,751.37
		273	-22,680.61	-116.70	0.00	0.00	0.00	-2,580.04
273	1	273	3,754.53	34.94	0.00	0.00	0.00	267.73
		274	-3,754.53	-34.94	0.00	0.00	0.00	-215.35
	2	273	22,680.61	192.45	0.00	0.00	0.00	2,580.03
		274	-22,994.35	-192.45	0.00	0.00	0.00	-2,294.22
274	1	274	3,754.52	33.54	0.00	0.00	0.00	215.35
		275	-3,754.52	-33.54	0.00	0.00	0.00	-165.05
	2	274	22,994.35	241.09	0.00	0.00	0.00	2,294.23
		275	-23,308.10	-241.09	0.00	0.00	0.00	-1,934.66
275	1	275	3,754.53	30.08	0.00	0.00	0.00	165.05
		276	-3,754.53	-30.08	0.00	0.00	0.00	-119.94
	2	275	23,308.05	254.18	0.00	0.00	0.00	1,934.66
		276	-23,621.80	-254.18	0.00	0.00	0.00	-1,534.80
276	1	276	3,754.54	25.57	0.00	0.00	0.00	119.94
		277	-3,754.54	-25.57	0.00	0.00	0.00	-81.58
	2	276	23,621.94	243.58	0.00	0.00	0.00	1,554.80
		277	-23,935.69	-243.58	0.00	0.00	0.00	-1,190.27
277	1	277	3,754.53	20.77	0.00	0.00	0.00	81.58
		278	-3,754.53	-20.77	0.00	0.00	0.00	-50.43

THE STUDY ON THE CONSTRUCTION OF THE BRIDGE OVER THE RIVER RUPSA IN KHULNA, PHASE - 2

Job No. :	Designed by :	Checked by :	Date :	January 24, 2000				
	2	277	23,935.62	218.88	0.00	0.00	0.00	1,190.27
		278	-24,249.36	-218.88	0.00	0.00	0.00	-862.35
278	1	278	3,754.53	16.17	0.00	0.00	0.00	50.43
		279	-3,754.53	-16.17	0.00	0.00	0.00	-26.18
	2	278	24,249.31	187.44	0.00	0.00	0.00	862.36
		279	-24,563.16	-187.44	0.00	0.00	0.00	-581.30
279	1	279	3,754.54	12.09	0.00	0.00	0.00	26.18
		280	-3,754.54	-12.09	0.00	0.00	0.00	-8.03
	2	279	24,563.14	154.58	0.00	0.00	0.00	581.30
		280	-24,876.89	-154.58	0.00	0.00	0.00	-349.30
280	1	280	3,754.53	8.72	0.00	0.00	0.00	8.03
		281	-3,754.53	-8.72	0.00	0.00	0.00	5.05
	2	280	24,876.91	123.95	0.00	0.00	0.00	349.30
		281	-25,190.66	-123.95	0.00	0.00	0.00	-163.10
281	1	281	3,754.53	6.08	0.00	0.00	0.00	-5.05
		282	-3,754.53	-6.08	0.00	0.00	0.00	14.18
	2	281	25,190.54	97.74	0.00	0.00	0.00	163.10
		282	-25,504.29	-97.74	0.00	0.00	0.00	-16.14
282	1	282	3,754.53	3.54	0.00	0.00	0.00	-14.18
		283	-3,754.53	-3.54	0.00	0.00	0.00	19.49
	2	282	25,504.42	70.10	0.00	0.00	0.00	16.14
		283	-25,818.17	-70.10	0.00	0.00	0.00	89.37
283	1	283	3,754.53	1.84	0.00	0.00	0.00	-19.49
		284	-3,754.53	-1.84	0.00	0.00	0.00	22.26
	2	283	25,818.16	49.92	0.00	0.00	0.00	-89.37
		284	-26,131.90	-49.92	0.00	0.00	0.00	164.59
284	1	284	3,754.54	-1.15	0.00	0.00	0.00	-22.26
		285	-3,754.54	1.15	0.00	0.00	0.00	20.54
	2	284	26,131.87	10.02	0.00	0.00	0.00	-164.59
		285	-26,445.62	-10.02	0.00	0.00	0.00	179.89
285	1	285	3,754.53	-2.60	0.00	0.00	0.00	-20.54
		286	-3,754.53	2.60	0.00	0.00	0.00	15.34
	2	285	26,445.58	-13.47	0.00	0.00	0.00	-179.89
		286	-26,863.91	13.47	0.00	0.00	0.00	153.20
286	1	286	3,754.54	-2.74	0.00	0.00	0.00	-15.34
		287	-3,754.54	2.74	0.00	0.00	0.00	9.86
	2	286	26,863.93	-21.63	0.00	0.00	0.00	-153.20
		287	-27,282.26	21.63	0.00	0.00	0.00	110.09
287	1	287	3,754.53	-2.25	0.00	0.00	0.00	-9.86
		288	-3,754.53	2.25	0.00	0.00	0.00	5.36
	2	287	27,282.28	-20.98	0.00	0.00	0.00	-110.09
		288	-27,700.62	20.98	0.00	0.00	0.00	68.20
288	1	288	3,754.53	-1.55	0.00	0.00	0.00	-5.36
		289	-3,754.53	1.55	0.00	0.00	0.00	2.26
	2	288	27,700.64	-16.31	0.00	0.00	0.00	-68.20
		289	-28,118.97	16.31	0.00	0.00	0.00	35.59
289	1	289	3,754.53	-0.95	0.00	0.00	0.00	-2.26
		290	-3,754.53	0.95	0.00	0.00	0.00	0.35
	2	289	28,118.94	-11.14	0.00	0.00	0.00	-35.59
		290	-28,537.27	11.14	0.00	0.00	0.00	13.30
290	1	290	3,754.53	-0.48	0.00	0.00	0.00	-0.35
		291	-3,754.53	0.48	0.00	0.00	0.00	-0.61
	2	290	28,537.34	-6.54	0.00	0.00	0.00	-13.30
		291	-28,955.67	6.54	0.00	0.00	0.00	0.19
291	1	291	3,754.53	-0.17	0.00	0.00	0.00	0.61
		292	-3,754.53	0.17	0.00	0.00	0.00	-0.95
	2	291	28,955.61	-3.10	0.00	0.00	0.00	-0.19
		292	-29,373.94	3.10	0.00	0.00	0.00	-6.04
292	1	292	3,754.53	0.01	0.00	0.00	0.00	0.95
		293	-3,754.53	-0.01	0.00	0.00	0.00	-0.93

THE STUDY ON THE CONSTRUCTION OF THE BRIDGE OVER THE RIVER RUPSA IN KHULNA, PHASE - 2

Job No. :	Designed by :	Checked by :	Date :					
			January 24, 2000					
	2	292	29,373.98	-0.88	0.00	0.00	0.00	6.04
		293	-29,792.32	0.88	0.00	0.00	0.00	-7.81
293	1	293	3,754.53	0.10	0.00	0.00	0.00	0.93
		294	-3,754.53	-0.10	0.00	0.00	0.00	-0.74
	2	293	29,792.26	0.34	0.00	0.00	0.00	7.81
		294	-30,210.59	-0.34	0.00	0.00	0.00	-7.14
294	1	294	3,734.53	0.12	0.00	0.00	0.00	0.74
		295	-3,754.53	-0.12	0.00	0.00	0.00	-0.50
	2	294	30,210.62	0.35	0.00	0.00	0.00	7.14
		295	-30,628.95	-0.35	0.00	0.00	0.00	-5.44
295	1	295	3,754.53	0.10	0.00	0.00	0.00	0.50
		296	-3,754.53	-0.10	0.00	0.00	0.00	-0.30
	2	295	30,628.94	0.93	0.00	0.00	0.00	5.44
		296	-31,047.27	-0.93	0.00	0.00	0.00	-3.58
296	1	296	3,754.53	0.08	0.00	0.00	0.00	0.30
		297	-3,754.53	-0.08	0.00	0.00	0.00	-0.14
	2	296	31,047.27	0.79	0.00	0.00	0.00	3.58
		297	-31,465.60	-0.79	0.00	0.00	0.00	-2.01
297	1	297	3,754.53	0.05	0.00	0.00	0.00	0.14
		298	-3,754.53	-0.05	0.00	0.00	0.00	-0.04
	2	297	31,465.60	0.57	0.00	0.00	0.00	2.01
		298	-31,883.93	-0.57	0.00	0.00	0.00	-0.88
298	1	298	3,754.53	0.03	0.00	0.00	0.00	0.04
		299	-3,754.53	-0.03	0.00	0.00	0.00	0.01
	2	298	31,883.94	0.35	0.00	0.00	0.00	0.88
		299	-32,302.27	-0.35	0.00	0.00	0.00	-0.17
299	1	299	3,754.53	0.01	0.00	0.00	0.00	-0.01
		300	-3,754.53	-0.01	0.00	0.00	0.00	0.04
	2	299	32,302.28	0.18	0.00	0.00	0.00	0.17
		300	-32,720.62	-0.18	0.00	0.00	0.00	0.19
300	1	300	3,754.53	0.00	0.00	0.00	0.00	-0.04
		301	-3,754.53	0.00	0.00	0.00	0.00	0.04
	2	300	32,720.58	0.07	0.00	0.00	0.00	-0.19
		301	-33,138.91	-0.07	0.00	0.00	0.00	0.33
301	1	301	3,754.53	0.00	0.00	0.00	0.00	-0.04
		302	-3,754.53	0.00	0.00	0.00	0.00	0.04
	2	301	33,138.95	0.00	0.00	0.00	0.00	-0.33
		302	-33,557.28	0.00	0.00	0.00	0.00	0.33
302	1	302	3,754.53	0.00	0.00	0.00	0.00	-0.04
		303	-3,754.53	0.00	0.00	0.00	0.00	0.03
	2	302	33,557.27	-0.03	0.00	0.00	0.00	-0.33
		303	-33,975.61	0.03	0.00	0.00	0.00	0.27
303	1	303	3,754.53	0.00	0.00	0.00	0.00	-0.03
		304	-3,754.53	0.00	0.00	0.00	0.00	0.02
	2	303	33,975.60	-0.04	0.00	0.00	0.00	-0.27
		304	-34,393.93	0.04	0.00	0.00	0.00	0.19
304	1	304	3,754.53	0.00	0.00	0.00	0.00	-0.02
		305	-3,754.53	0.00	0.00	0.00	0.00	0.01
	2	304	34,393.93	-0.04	0.00	0.00	0.00	-0.19
		305	-34,812.27	0.04	0.00	0.00	0.00	0.11
305	1	305	3,754.53	0.00	0.00	0.00	0.00	-0.01
		306	-3,754.53	0.00	0.00	0.00	0.00	0.00
	2	305	34,812.27	-0.03	0.00	0.00	0.00	-0.11
		306	-35,230.61	0.03	0.00	0.00	0.00	0.05
306	1	306	3,754.53	0.00	0.00	0.00	0.00	0.00
		307	-3,754.53	0.00	0.00	0.00	0.00	0.00
	2	306	35,230.60	-0.03	0.00	0.00	0.00	-0.05
		307	-35,648.93	0.03	0.00	0.00	0.00	0.00
307	1	307	3,754.53	0.00	0.00	0.00	0.00	0.00
		308	-3,754.53	0.00	0.00	0.00	0.00	-0.01

THE STUDY ON THE CONSTRUCTION OF THE BRIDGE OVER THE RIVER RUPSA IN KHULNA, PHASE - 2

Job No. :	Designed by :	Checked by :	Date :	January 24, 2000				
	2	307	35,648.93	-0.02	0.00	0.00	0.00	0.00
		308	-36,067.27	0.02	0.00	0.00	0.00	-0.05
308	1	308	3,754.53	0.00	0.00	0.00	0.00	0.01
		309	-3,754.53	0.00	0.00	0.00	0.00	-0.01
	2	308	36,067.27	-0.02	0.00	0.00	0.00	0.05
		309	-36,381.01	0.02	0.00	0.00	0.00	-0.08
309	1	242	-2,345.92	23.99	0.00	0.00	0.00	950.13
		310	2,345.92	-23.99	0.00	0.00	0.00	-913.66
	2	242	24,408.87	649.79	0.00	0.00	0.00	14,854.97
		310	-24,722.61	-649.79	0.00	0.00	0.00	-13,908.19
310	1	310	-2,345.91	24.04	0.00	0.00	0.00	913.67
		311	2,345.91	-24.04	0.00	0.00	0.00	-693.36
	2	310	24,722.24	649.78	0.00	0.00	0.00	13,908.18
		311	-26,604.74	-649.78	0.00	0.00	0.00	-8,129.95
311	1	311	-2,345.90	24.04	0.00	0.00	0.00	693.37
		312	2,345.90	-24.04	0.00	0.00	0.00	-333.45
	2	311	26,604.78	649.77	0.00	0.00	0.00	8,129.96
		312	-29,742.27	-649.77	0.00	0.00	0.00	1,716.35
312	1	312	-2,345.90	25.69	0.00	0.00	0.00	355.45
		314	2,345.90	-25.69	0.00	0.00	0.00	-294.87
	2	312	29,742.13	534.39	0.00	0.00	0.00	-1,716.34
		314	-30,055.87	-534.39	0.00	0.00	0.00	2,532.18
313	1	314	-2,345.92	27.42	0.00	0.00	0.00	294.87
		315	2,345.92	-27.42	0.00	0.00	0.00	-253.71
	2	314	30,036.20	345.80	0.00	0.00	0.00	-2,532.15
		315	-30,369.95	-345.80	0.00	0.00	0.00	3,064.10
314	1	315	-2,345.89	28.00	0.00	0.00	0.00	253.71
		316	2,345.89	-28.00	0.00	0.00	0.00	-211.69
	2	315	30,369.77	196.43	0.00	0.00	0.00	-3,064.13
		316	-30,683.52	-196.43	0.00	0.00	0.00	3,370.73
315	1	316	-2,345.91	27.77	0.00	0.00	0.00	211.69
		317	2,345.91	-27.77	0.00	0.00	0.00	-170.02
	2	316	30,683.45	82.16	0.00	0.00	0.00	-3,370.77
		317	-30,997.20	-82.16	0.00	0.00	0.00	3,504.55
316	1	317	-2,345.90	27.02	0.00	0.00	0.00	170.02
		318	2,345.90	-27.02	0.00	0.00	0.00	-129.49
	2	317	30,997.08	-1.64	0.00	0.00	0.00	-3,504.57
		318	-31,310.82	1.64	0.00	0.00	0.00	3,511.10
317	1	318	-2,345.90	25.68	0.00	0.00	0.00	129.49
		319	2,345.90	-25.68	0.00	0.00	0.00	-90.97
	2	318	31,310.66	-76.55	0.00	0.00	0.00	-3,511.13
		319	-31,624.41	76.55	0.00	0.00	0.00	3,403.77
318	1	319	-2,345.94	21.69	0.00	0.00	0.00	90.97
		320	2,345.94	-21.69	0.00	0.00	0.00	-58.43
	2	319	31,624.62	-205.03	0.00	0.00	0.00	-3,403.78
		320	-31,938.37	205.03	0.00	0.00	0.00	3,102.06
319	1	320	-2,345.90	17.77	0.00	0.00	0.00	58.43
		321	2,345.90	-17.77	0.00	0.00	0.00	-31.77
	2	320	31,938.30	-277.04	0.00	0.00	0.00	-3,102.07
		321	-32,252.05	277.04	0.00	0.00	0.00	2,690.88
320	1	321	-2,345.90	13.12	0.00	0.00	0.00	31.78
		322	2,345.90	-13.12	0.00	0.00	0.00	-12.10
	2	321	32,252.22	-316.06	0.00	0.00	0.00	-2,690.89
		322	-32,565.97	316.06	0.00	0.00	0.00	2,219.88
321	1	322	-2,345.92	9.11	0.00	0.00	0.00	12.10
		323	2,345.92	-9.11	0.00	0.00	0.00	1.57
	2	322	32,565.86	-317.20	0.00	0.00	0.00	-2,219.88
		323	-32,879.61	317.20	0.00	0.00	0.00	1,746.08
322	1	323	-2,345.91	5.83	0.00	0.00	0.00	-1.57
		324	2,345.91	-5.83	0.00	0.00	0.00	10.30

THE STUDY ON THE CONSTRUCTION OF THE BRIDGE OVER THE RIVER RUPSA IN KHULNA, PHASE - 2

Job No. :	Designed by :	Checked by :	Date :	January 24, 2000				
	2	323	32,879.89	-294.11	0.00	0.00	0.00	-1,746.08
		324	-33,193.64	294.11	0.00	0.00	0.00	1,306.04
323	1	324	-2,345.92	3.26	0.00	0.00	0.00	-10.30
		325	2,345.92	-3.26	0.00	0.00	0.00	15.19
	2	324	33,193.44	-257.56	0.00	0.00	0.00	-1,306.05
		325	-33,507.19	257.56	0.00	0.00	0.00	920.17
324	1	325	-2,345.91	1.35	0.00	0.00	0.00	-15.19
		326	2,345.91	-1.35	0.00	0.00	0.00	17.20
	2	325	33,507.34	-215.63	0.00	0.00	0.00	-920.16
		326	-33,821.09	215.63	0.00	0.00	0.00	596.70
325	1	326	-2,345.90	0.00	0.00	0.00	0.00	-17.20
		327	2,345.90	0.00	0.00	0.00	0.00	17.20
	2	326	33,820.85	-174.05	0.00	0.00	0.00	-596.69
		327	-34,134.59	174.05	0.00	0.00	0.00	335.28
326	1	327	-2,345.91	-0.88	0.00	0.00	0.00	-17.20
		328	2,345.91	0.88	0.00	0.00	0.00	15.88
	2	327	34,134.57	-136.56	0.00	0.00	0.00	-335.28
		328	-34,448.31	136.56	0.00	0.00	0.00	129.90
327	1	328	-2,345.91	-1.40	0.00	0.00	0.00	-15.88
		329	2,345.91	1.40	0.00	0.00	0.00	13.78
	2	328	34,448.46	-105.27	0.00	0.00	0.00	-129.91
		329	-34,762.21	105.27	0.00	0.00	0.00	-28.60
328	1	329	-2,345.90	-1.74	0.00	0.00	0.00	-13.78
		330	2,345.90	1.74	0.00	0.00	0.00	11.16
	2	329	34,762.20	-72.96	0.00	0.00	0.00	28.60
		330	-35,075.95	72.96	0.00	0.00	0.00	-138.64
329	1	330	-2,345.90	-1.85	0.00	0.00	0.00	-11.16
		331	2,345.90	1.85	0.00	0.00	0.00	8.39
	2	330	35,075.99	-49.87	0.00	0.00	0.00	138.64
		331	-35,389.74	49.87	0.00	0.00	0.00	-213.98
330	1	331	-2,345.91	-1.73	0.00	0.00	0.00	-8.39
		313	2,345.91	1.73	0.00	0.00	0.00	5.79
	2	331	35,389.69	-5.32	0.00	0.00	0.00	213.98
		313	-35,703.44	5.32	0.00	0.00	0.00	-222.39
331	1	313	-2,345.91	-1.39	0.00	0.00	0.00	-5.79
		333	2,345.91	1.39	0.00	0.00	0.00	3.02
	2	313	35,703.41	19.94	0.00	0.00	0.00	222.39
		333	-36,121.74	-19.94	0.00	0.00	0.00	-182.91
332	1	333	-2,345.90	-0.94	0.00	0.00	0.00	-3.02
		334	2,345.90	0.94	0.00	0.00	0.00	1.13
	2	333	36,121.73	27.61	0.00	0.00	0.00	182.91
		334	-36,540.07	-27.61	0.00	0.00	0.00	-127.90
333	1	334	-2,345.91	-0.55	0.00	0.00	0.00	-1.13
		335	2,345.91	0.55	0.00	0.00	0.00	0.03
	2	334	36,540.13	25.52	0.00	0.00	0.00	127.90
		335	-36,958.46	-25.52	0.00	0.00	0.00	-76.96
334	1	335	-2,345.91	-0.25	0.00	0.00	0.00	-0.03
		336	2,345.91	0.25	0.00	0.00	0.00	-0.47
	2	335	36,958.47	19.24	0.00	0.00	0.00	76.96
		336	-37,376.80	-19.24	0.00	0.00	0.00	-38.50
335	1	336	-2,345.91	-0.07	0.00	0.00	0.00	0.47
		337	2,345.91	0.07	0.00	0.00	0.00	-0.61
	2	336	37,376.74	12.80	0.00	0.00	0.00	38.50
		337	-37,795.07	-12.80	0.00	0.00	0.00	-12.88
336	1	337	-2,345.91	0.02	0.00	0.00	0.00	0.61
		338	2,345.91	-0.02	0.00	0.00	0.00	-0.56
	2	337	37,795.09	7.28	0.00	0.00	0.00	12.88
		338	-38,213.42	-7.28	0.00	0.00	0.00	1.72
337	1	338	-2,345.91	0.07	0.00	0.00	0.00	0.56
		339	2,345.91	-0.07	0.00	0.00	0.00	-0.43

THE STUDY ON THE CONSTRUCTION OF THE BRIDGE OVER THE RIVER RUPSA IN KHULNA, PHASE - 2

Job No. :	Designed by :	Checked by :	Date :	January 24, 2000				
	2	338	38,213.43	3.26	0.00	0.00	0.00	-1.72
		339	-38,631.76	-3.26	0.00	0.00	0.00	8.28
338	1	339	-2,345.91	0.07	0.00	0.00	0.00	0.43
		340	2,345.91	-0.07	0.00	0.00	0.00	-0.28
	2	339	38,631.79	0.73	0.00	0.00	0.00	-8.28
		340	-39,050.12	-0.73	0.00	0.00	0.00	9.78
339	1	340	-2,345.91	0.06	0.00	0.00	0.00	0.28
		341	2,345.91	-0.06	0.00	0.00	0.00	-0.16
	2	340	39,050.15	-0.60	0.00	0.00	0.00	-9.78
		341	-39,468.48	0.60	0.00	0.00	0.00	8.60
340	1	341	-2,345.91	0.04	0.00	0.00	0.00	0.16
		342	2,345.91	-0.04	0.00	0.00	0.00	-0.07
	2	341	39,468.44	-1.12	0.00	0.00	0.00	-8.60
		342	-39,886.77	1.12	0.00	0.00	0.00	6.37
341	1	342	-2,345.91	0.03	0.00	0.00	0.00	0.07
		343	2,345.91	-0.03	0.00	0.00	0.00	-0.01
	2	342	39,886.78	-1.15	0.00	0.00	0.00	-6.37
		343	-40,305.11	1.15	0.00	0.00	0.00	4.09
342	1	343	-2,345.91	0.01	0.00	0.00	0.00	-0.01
		344	2,345.91	-0.01	0.00	0.00	0.00	0.01
	2	343	40,305.12	-0.94	0.00	0.00	0.00	-4.09
		344	-40,723.45	0.94	0.00	0.00	0.00	2.22
343	1	344	-2,345.91	0.01	0.00	0.00	0.00	-0.01
		345	2,345.91	-0.01	0.00	0.00	0.00	0.03
	2	344	40,723.43	-0.66	0.00	0.00	0.00	-2.22
		345	-41,141.76	0.66	0.00	0.00	0.00	0.90
344	1	345	-2,345.91	0.00	0.00	0.00	0.00	-0.03
		346	2,345.91	0.00	0.00	0.00	0.00	0.03
	2	345	41,141.78	-0.40	0.00	0.00	0.00	-0.90
		346	-41,560.11	0.40	0.00	0.00	0.00	0.11
345	1	346	-2,345.91	0.00	0.00	0.00	0.00	-0.03
		347	2,345.91	0.00	0.00	0.00	0.00	0.02
	2	346	41,560.13	-0.20	0.00	0.00	0.00	-0.11
		347	-41,978.47	0.20	0.00	0.00	0.00	-0.29
346	1	347	-2,345.91	0.00	0.00	0.00	0.00	-0.02
		348	2,345.91	0.00	0.00	0.00	0.00	-0.01
	2	347	41,978.42	-0.07	0.00	0.00	0.00	0.29
		348	-42,396.76	0.07	0.00	0.00	0.00	-0.42
347	1	348	-2,345.91	0.00	0.00	0.00	0.00	-0.01
		349	2,345.91	0.00	0.00	0.00	0.00	0.01
	2	348	42,396.79	0.01	0.00	0.00	0.00	0.42
		349	-42,815.12	-0.01	0.00	0.00	0.00	-0.41
348	1	349	-2,345.91	0.00	0.00	0.00	0.00	-0.01
		350	2,345.91	0.00	0.00	0.00	0.00	0.00
	2	349	42,815.13	0.04	0.00	0.00	0.00	0.41
		350	-43,233.46	-0.04	0.00	0.00	0.00	-0.32
349	1	350	-2,345.91	0.00	0.00	0.00	0.00	0.00
		351	2,345.91	0.00	0.00	0.00	0.00	0.00
	2	350	43,233.43	0.05	0.00	0.00	0.00	0.32
		351	-43,651.76	-0.05	0.00	0.00	0.00	-0.22
350	1	351	-2,345.91	0.00	0.00	0.00	0.00	0.00
		352	2,345.91	0.00	0.00	0.00	0.00	0.00
	2	351	43,651.78	0.05	0.00	0.00	0.00	0.22
		352	-44,070.11	-0.05	0.00	0.00	0.00	-0.13
351	1	352	-2,345.91	0.00	0.00	0.00	0.00	0.00
		353	2,345.91	0.00	0.00	0.00	0.00	0.00
	2	352	44,070.11	0.04	0.00	0.00	0.00	0.13
		353	-44,488.44	-0.04	0.00	0.00	0.00	-0.05
352	1	353	-2,345.91	0.00	0.00	0.00	0.00	0.00
		354	2,345.91	0.00	0.00	0.00	0.00	0.00

**THE STUDY ON THE CONSTRUCTION OF THE BRIDGE
OVER THE RIVER RUPSA IN KHULNA, PHASE - 2**

Job No. :	Designed by :	Checked by :	Date :	January 24, 2000				
	2	353	44,488.44	0.03	0.00	0.00	0.00	0.03
		354	-44,906.77	-0.03	0.00	0.00	0.00	0.01
353	1	354	-2,345.91	0.00	0.00	0.00	0.00	0.00
		332	2,345.91	0.00	0.00	0.00	0.00	0.00
	2	354	-44,906.77	0.03	0.00	0.00	0.00	-0.01
		332	-45,325.10	-0.03	0.00	0.00	0.00	0.06
354	1	332	-2,345.91	0.00	0.00	0.00	0.00	0.00
		355	2,345.91	0.00	0.00	0.00	0.00	0.00
	2	332	45,325.10	0.03	0.00	0.00	0.00	-0.06
		355	-45,638.85	-0.03	0.00	0.00	0.00	0.10